



TRANSPORTATION CABINET

Frankfort, Kentucky 40622
www.transportation.ky.gov/

Steven L. Beshear
Governor

Michael W. Hancock, P.E.
Secretary

February 13, 2012

CALL NO. 100
CONTRACT ID NO. 121002
ADDENDUM # 1

Subject: Campbell County, IM NH 4714 (034)
Letting February 24, 2012

- (1) Revised - Plan Sheets - R2ae, R145, & R146
- (2) Revised - Special Notes - Pages 178-182 of 321
- (3) Added - Special Notes - Pages 188(a)-188(b) of 321
- (4) Revised - Bid Items - Pages 308-321 of 321

Proposal revisions are available at <http://transportation.ky.gov/contract/>.
Plan revisions are available at <http://www.lynnimaging.com/kytransportation/>.

If you have any questions, please contact us at 502-564-3500.

Sincerely,

A handwritten signature in blue ink that reads "Ryan Griffith".

Ryan Griffith
Director
Division of Construction Procurement

RG:ks
Enclosures



An Equal Opportunity Employer M/F/D

GENERAL SUMMARY

COUNTY OF	ITEM NO.	SHEET NO.
CAMPBELL	6-2021.00	R20e

ITEM	DESCRIPTION	UNIT	I-471	I-275 WB	I-275 RAMP A	US 27 RAMP A	US 27 RAMP B	US 27 RAMP C	US 27 RAMP D	GRAND AVENUE RAMP E	GRAND AVENUE RAMP EE	GRAND AVENUE RAMP F	GRAND AVENUE RAMP H	GRAND AVENUE RAMP K	MEMORIAL PARKWAY RAMP A	MEMORIAL PARKWAY RAMP D	MEMORIAL PARKWAY RAMP M	MEMORIAL PARKWAY RAMP N	KY 8 RAMP A	KY 8 RAMP B	KY 8 RAMP K	KY 8 RAMP L	MAINT. OF TRAFFIC	PROJECT TOTAL
6417	FLEXIBLE DELINEATOR POST-W	EACH	233	9	30	16	14	10	12	30	4	23	7	18	12	9	8	6	8	7	12	12		480
6418	FLEXIBLE DELINEATOR POST-Y	EACH			30	16	14	10	12	30	4	23	7	18	12	9	8	6	8	7	12	12		238
6511	PAVE STRIPING-TEMP PAINT-6 IN	LIN FT																					380000	380000 ^⑦
6513	PAVE STRIPING-TEMP PAINT-12 IN	LIN FT																					16400	16400 ^⑦
6549	PAVE STRIPING-TEMP REM TAPE-B	LIN FT																						25000 ^⑦
6550	PAVE STRIPING-TEMP REM TAPE-W	LIN FT																						25000 ^⑦
6551	PAVE STRIPING-TEMP REM TAPE-Y	LIN FT																						25000 ^⑦
6568	PAVEMENT MARKING-THERMO STOP BAR-24 IN	LIN FT				27		43		18	15		20			18		15				35		191
6573	PAVEMENT MARKING-THERMO STRAIGHT ARROW	EACH						1			1													2
6574	PAVEMENT MARKING-THERMO CURVE ARROW	EACH				4		4														12		20
6576	PAVEMENT MARKING-THERMO ONLY	EACH				2		2														3		7
6585	PAVEMENT MARKER TY IVA-MW TEMP	EACH																					4352	4352 ^⑦
6586	PAVEMENT MARKER TY IVA-MY TEMP	EACH																					8632	8632 ^⑦
6592	PAVEMENT MARKER TYPE V-B W/R	EACH	1753	62	28	3		12				21												1879
6593	PAVEMENT MARKER TYPE V-B Y/R	EACH	30		37	45	36	25	31	71	12	60	19	46	32	23	21	19	27	19	35	63		651
8001	STRUCTURE EXCAVATION - COMMON ^①	CU YD		318																				318
8100	CONCRETE - CLASS A	CU YD																						4.66 ^④
8150	STEEL REINFORCEMENT	LB																						178 ^⑤
20410ED	MAINTAIN LIGHTING	LP SUM																						1
23158ES505	DETECTABLE WARNINGS	SO FT									32	44									39			115
21935EN	REMOVE CONCRETE MEDIAN BARRIER	LIN FT	2365																		151			2516
23143ED	K.P.D.E.S. PERMIT AND TEMPORARY EROSION CONTROL ^②	LP SUM																						1
23237ENIOW	WATERBLAST STRIPE REMOVAL	LIN FT																					385000	385000 ^⑦
23864EC	CHANNEL LINING CLASS III-MOD ^⑥	TON		225																				365 ^③
24189ER	DURABLE WATERBORNE MARKING-6 IN W	LIN FT	93322	3188	2780	2105	2300	1631	1830	3621	120	4065	1200	2250	2310	1340	1560	1275	1400	800	1600	3330		132027
24190ER	DURABLE WATERBORNE MARKING-6 IN Y	LIN FT	61293		1475	1800	1450	1000	1241	2856	475	2400	750	1850	1260	930	840	740	1090	760	1400	2500		86110
24191ER	DURABLE WATERBORNE MARKING-12 IN W	LIN FT	21195	1000																			1600 ^⑧	23795
24458EC	UTILITY LINE HANGER FOR BRIDGES ^②	EACH		2																				2

- ① FOR GRAVITY RETAINING WALLS - SEE SHEETS R100 AND R135
- ② SEE SPECIAL NOTE IN THE PROPOSAL
- ③ 140 TONS CARRIED FORWARD FROM THE GUARDRAIL SUMMARY
- ④ 2.98 CU YD CARRIED FORWARD FROM THE GUARDRAIL SUMMARY, 1.68 CU YD CARRIED FORWARD FROM THE PIPE DRAINAGE SUMMARY
- ⑤ 163 LBS CARRIED FORWARD FROM THE GUARDRAIL SUMMARY, 15 LBS CARRIED FORWARD FROM THE PIPE DRAINAGE SUMMARY
- ⑥ SEE SHEET R136 FOR DETAIL DRAWING
- ⑦ INCLUDES QUANTITIES FOR MAINTENANCE OF TRAFFIC WORK IN OHIO
- ⑧ FOR CROSS-HATCHING REPLACEMENT IN RAMP GORE AREAS IN OHIO

FILE NAME: P:\CIVIL\14716-2021.00\CONTRACT PLANS AND PROPOSAL REVISIONS AFTER SUBMITTAL\PLAN REVISION 4.R002AESU.DGN
 USER: tvonbehren
 DATE PLOTTED: February 10, 2012
 E-SHEET NAME:
 MicroStation v8.11.7.443

GENERAL SUMMARY

△ REVISED 2-10-12

COUNTY OF	ITEM NO.	SHEET NO.
CAMPBELL	6-2021.00	R20e

ITEM	DESCRIPTION	UNIT	I-471	I-275 WB	I-275 RAMP A	US 27 RAMP A	US 27 RAMP B	US 27 RAMP C	US 27 RAMP D	GRAND AVENUE RAMP E	GRAND AVENUE RAMP EE	GRAND AVENUE RAMP F	GRAND AVENUE RAMP H	GRAND AVENUE RAMP K	MEMORIAL PARKWAY RAMP A	MEMORIAL PARKWAY RAMP D	MEMORIAL PARKWAY RAMP M	MEMORIAL PARKWAY RAMP N	KY 8 RAMP A	KY 8 RAMP B	KY 8 RAMP K	KY 8 RAMP L	MAINT. OF TRAFFIC	PROJECT TOTAL
6417	FLEXIBLE DELINEATOR POST-W	EACH	233	9	30	16	14	10	12	30	4	23	7	18	12	9	8	6	8	7	12	12		480
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CAMPBELL	6-2021.00	R145

TRAFFIC CONTROL GENERAL

EXCEPT AS PROVIDED HEREIN, MAINTAIN AND CONTROL TRAFFIC IN ACCORDANCE WITH THE 2008 STANDARD SPECIFICATIONS, THE STANDARD DRAWINGS AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), CURRENT EDITIONS. EXCEPT FOR THE ROADWAY AND TRAFFIC CONTROL BID ITEMS LISTED, ALL ITEMS OF WORK NECESSARY TO MAINTAIN AND CONTROL TRAFFIC WILL BE PAID AT THE LUMP SUM BID PRICE TO "MAINTAIN AND CONTROL TRAFFIC". ALL LANE CLOSURES USED ON THE PROJECT WILL BE IN COMPLIANCE WITH THE APPROPRIATE STANDARD DRAWINGS.

CONTRARY TO SECTION 106.01, TRAFFIC CONTROL DEVICES USED ON THIS PROJECT MAY BE NEW, OR USED IN LIKE NEW CONDITION, AT THE BEGINNING OF THE WORK AND MAINTAINED IN LIKE NEW CONDITION UNTIL COMPLETION OF THE WORK. TRAFFIC CONTROL DEVICES WILL CONFORM TO CURRENT MUTCD SPECIFICATIONS.

DURING CONSTRUCTION, SIGNS AND PORTABLE CHANGEABLE MESSAGE SIGNS WILL BE INSTALLED AT LOCATIONS SPECIFIED BY THE ENGINEER. SEE THE MAINTENANCE OF TRAFFIC CONSTRUCTION PHASING PLAN FOR TRAFFIC CONTROL DETAILS DURING EACH PHASE OF CONSTRUCTION. THE MINIMUM LANE WIDTH FOR THIS PROJECT SHALL BE 11 FEET. DURING CONSTRUCTION, THE EXISTING POSTED 65 MPH SPEED LIMIT SHALL BE REDUCED TO 55 MPH.

IN THESE NOTES, A WORK WEEK SHALL BE FROM 6 AM ON MONDAY TO 8 PM ON FRIDAY AND A WEEKEND SHALL BE FROM 8 PM ON FRIDAY TO 6 AM THE FOLLOWING MONDAY.

MAINTENANCE OF TRAFFIC ACTIVITIES WILL BE REQUIRED ON THE OHIO SIDE OF THE TWIN I-471 BRIDGES OVER THE OHIO RIVER AT THE NORTH END OF THIS PROJECT.

DRUMS OR TEMPORARY CONCRETE BARRIER WALL ARE REQUIRED FOR ALL TRAFFIC OPERATIONS IN KENTUCKY AND OHIO. THE USE OF CONES WILL NOT BE PERMITTED.

NO PAYMENT WILL BE AWARDED FOR VALUE ENGINEERING THE MAINTENANCE OF TRAFFIC PLANS.

CONSTRUCTION PHASING AND SEQUENCE OF CONSTRUCTION

CONSTRUCTION PHASING AND THE SEQUENCE OF CONSTRUCTION WILL BE MAINTAINED AS SHOWN IN THE PLANS AND THE MAINTENANCE OF TRAFFIC NOTES UNLESS OTHERWISE APPROVED BY THE ENGINEER.

ALL I-471 NORTHBOUND WORK AND ASSOCIATED RAMP WORK AT THE I-275, US 27, GRAND AVENUE, MEMORIAL PARKWAY AND KY 8 INTERCHANGES IS TO BE COMPLETED BY DECEMBER 1, 2012. NO TRAFFIC IMPACTS WILL BE PERMITTED ON I-471 SOUTHBOUND OR ASSOCIATED RAMPS PRIOR TO COMPLETION OF ALL I-471 NORTHBOUND WORK AND ASSOCIATED RAMPS.

NO TRAFFIC IMPACTS WILL BE ALLOWED ON I-471, AND ASSOCIATED RAMPS, IN EITHER DIRECTION FROM DECEMBER 1, 2012 UNTIL APRIL 1, 2013.

CONSTRUCTION SIGNING IS NOT TO BE INSTALLED ON I-471 SOUTHBOUND PRIOR TO BEGINNING WORK ON THE SOUTHBOUND SIDE. SIGNS FOR TEMPORARY SHOULDER CLOSURES ON I-471 SOUTHBOUND ARE TO BE INSTALLED, THEN REMOVED WHEN THE SHOULDER IS REOPENED.

IF THE ASPHALT PAVEMENT ALTERNATE IS CHOSEN, THE ASPHALT SURFACE COURSE IS NOT TO BE PLACED UNTIL ALL AREAS, OF THE DIRECTION UNDER CONSTRUCTION, HAVE THEIR FINAL BASE COURSE. FINAL STRIPING IS THEN TO BE PLACED ON THE SURFACE COURSE.

ALL I-471 SOUTHBOUND WORK AND ASSOCIATED RAMP WORK AT THE I-275, US 27, GRAND AVENUE, MEMORIAL PARKWAY AND KY 8 INTERCHANGES IS TO BE COMPLETED BY DECEMBER 1, 2013.

WORK ON I-471 NB AND SB FROM THE BEGINNING OF THE PROJECT AT STA 15+09.42 (US 27) TO I-471 NB STA 53+00 AND I-471 SB STA 49+00 IS ONLY TO BE DONE DURING THE SUMMER MONTHS WHEN NORTHERN KENTUCKY UNIVERSITY IS NOT IN A SPRING OR FALL SEMESTER. FOR I-471 NB, WORK MAY NOT BEGIN PRIOR TO MAY 7, 2012 AND MUST BE COMPLETED BY AUGUST 15, 2012. FOR I-471 SB, WORK MAY NOT BEGIN UNTIL AFTER THE 2013 SPRING COMMENCEMENT CEREMONY AND MUST BE COMPLETED 10 DAYS PRIOR TO START OF FALL CLASSES IN 2013 (SPECIFIC DATES TO BE DETERMINED). THE CONTRACTOR IS TO NOTIFY THE ENGINEER AT LEAST 14 DAYS PRIOR TO BEGINNING WORK ON EITHER I-471 NB OR SB.

THE CONTRACTOR MUST NOTIFY THE ENGINEER 14 DAYS PRIOR TO MAJOR TRAFFIC CHANGES OR RAMP CLOSURES. SEE THE SPECIAL NOTE FOR FIXED COMPLETION DATE AND LIQUIDATED DAMAGES FOR ADDITIONAL DETAILS.

RAMP CLOSURES

ONLY ONE RAMP MAY BE CLOSED AT A TIME UNLESS SPECIFICALLY NOTED OTHERWISE IN THESE PLANS OR APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL PLACE A PORTABLE CHANGEABLE MESSAGE SIGN WARNING OF PENDING RAMP CLOSURE IN THE AREA OF EACH RAMP TO BE CLOSED AT LEAST 10 DAYS PRIOR TO ITS CLOSURE.

THE CONTRACTOR WILL BE PERMITTED TO CLOSE RAMPS B, C, D, E AND G AT THE I-471/I-275 INTERCHANGE FOR A PERIOD OF 9 CONSECUTIVE DAYS (TWO WEEKENDS AND ONE WORK WEEK) AND ALL RAMP WORK IS TO BE COMPLETED DURING THAT TIME. THE CONTRACTOR WILL BE PERMITTED TO CLOSE RAMP A AT THE I-275/I-471 INTERCHANGE FOR A PERIOD OF 16 CONSECUTIVE DAYS (THREE WEEKENDS AND TWO WORK WEEKS) AND ALL RAMP WORK IS TO BE COMPLETED BY THE END OF THE PERIOD INCLUDING THE SEALING OF SHOULDERS. THE CLOSURE OF RAMPS E AND G MUST OCCUR DURING THE ACCELERATED CONSTRUCTION OF THE SECTION OF I-471 NB THEY JOIN AS SHOWN IN THE NOTE TITLED "ACCELERATED CONSTRUCTION".

THE TWO LANE RAMPS AT THE I-471/I-275 INTERCHANGE, RAMPS F AND H, ARE NOT TO BE CLOSED. TRAFFIC IS TO BE REDUCED TO ONE LANE AND SHIFTED PARTIALLY ONTO THE SHOULDER WHILE WORK IS BEING PERFORMED ON HALF OF THE RAMP, THEN SHIFTED TO THE OPPOSITE SIDE TO PERFORM THE REMAINING WORK ON THE RAMP. SEE THE MAINTENANCE OF TRAFFIC PLANS, TYPICAL SECTIONS AND NOTES FOR DETAILS ON CONSTRUCTION PHASING FOR THESE RAMPS.

THE CONTRACTOR WILL BE PERMITTED TO CLOSE ALL FOUR US 27 RAMPS FOR A PERIOD OF 16 CONSECUTIVE DAYS (THREE WEEKENDS AND TWO WORK WEEKS) AND ALL RAMP WORK IS TO BE COMPLETED DURING THAT TIME INCLUDING THE SEALING OF SHOULDERS.

THE CONTRACTOR WILL BE PERMITTED TO CLOSE GRAND AVENUE (GA) RAMPS E, EE, AND F FOR A PERIOD OF 16 CONSECUTIVE DAYS (THREE WEEKENDS AND TWO WORK WEEKS) AND ALL RAMP WORK IS TO BE COMPLETED DURING THAT TIME.

THE CONTRACTOR WILL BE PERMITTED TO CLOSE MEMORIAL PARKWAY (MP) RAMPS M AND N FOR A PERIOD OF 16 CONSECUTIVE DAYS (THREE WEEKENDS AND TWO WORK WEEKS) AND ALL RAMP WORK IS TO BE COMPLETED DURING THAT TIME.

RAMP CLOSURES (CONTINUED)

THE CONTRACTOR WILL BE PERMITTED TO CLOSE GA RAMP K AND MP RAMP D ON I-471 NORTHBOUND SIMULTANEOUSLY FOR 16 DAYS (THREE WEEKENDS AND TWO WORK WEEKS) DURING PHASE 3. THE CONTRACTOR WILL ALSO BE PERMITTED TO CLOSE GA RAMP H AND MP RAMP A ON I-471 SOUTHBOUND SIMULTANEOUSLY FOR 16 DAYS (THREE WEEKENDS AND TWO WORK WEEKS) DURING PHASE 3. DURING THE 16 DAYS THE RAMPS ON ONE DIRECTION OF I-471 ARE CLOSED, ALL WORK ON THE MAINLINE IN PHASE 3, INCLUDING THE WORK ON THE BRIDGE OVER CHESAPEAKE AVENUE, MUST BE COMPLETED.

THE CONTRACTOR WILL BE PERMITTED TO CLOSE KY 8 RAMPS A, B, K AND L (RAMPS A AND L SIMULTANEOUSLY, RAMPS B AND K SIMULTANEOUSLY) FOR A PERIOD OF 16 CONSECUTIVE DAYS (THREE WEEKENDS AND TWO WORK WEEKS) IN PHASE 3 AND ALL RAMP WORK IS TO BE COMPLETED BY THE END OF THE PERIOD. THESE RAMPS SHALL BE CLOSED DURING THE SAME PERIOD AS THE ACCELERATED CONSTRUCTION SECTION OF I-471 FROM THE I-471 TWIN BRIDGES OVER 6TH STREET TO THE END OF THE PROJECT IS PERFORMED.

COMPLETION DATE DISINCENTIVE

SEE THE SPECIAL NOTE FOR FIXED COMPLETION DATES AND LIQUIDATED DAMAGES FOR DETAILS CONCERNING LIQUIDATED DAMAGES TO BE CHARGED TO THE CONTRACTOR FOR FAILURE TO COMPLETE ANY WORK BY THE SPECIFIED COMPLETION DATE.

ACCELERATED CONSTRUCTION

FOR EACH OF THE FOLLOWING ACCELERATED CONSTRUCTION AREAS THE CONTRACTOR WILL HAVE 16 CONSECUTIVE DAYS (THREE WEEKENDS AND TWO WORK WEEKS) TO COMPLETE ALL WORK, INCLUDING ANY BRIDGE WORK, SHOWN IN THE PLANS FOR THAT AREA (WITH THE EXCEPTION OF THE ASPHALT SURFACE COURSE WHEN ASPHALT IS CHOSEN AS THE ALTERNATE PAVEMENT TYPE OR WHERE NO ALTERNATE PAVEMENT IS SHOWN):

- ALL PHASE 2 WORK ON I-471 NB FROM US 27 TO I-275 NB STATION 53+00
- ALL PHASE 3 WORK ON I-471 NB FROM US 27 TO I-275 NB STATION 53+00
- ALL PHASE 2 WORK ON I-471 NB FROM THE SOUTH END OF THE I-471 BRIDGE OVER 6TH STREET TO THE NORTH LIMITS OF THE PROJECT ON THE OHIO RIVER BRIDGE INCLUDING ALL BRIDGE WORK ON BOTH BRIDGES.
- ALL PHASE 2 WORK ON I-471 SB FROM THE SOUTH END OF THE I-471 BRIDGE OVER 6TH STREET TO THE NORTH LIMITS OF THE PROJECT ON THE OHIO RIVER BRIDGE INCLUDING ALL BRIDGE WORK ON BOTH BRIDGES. RAMP CLOSURES/LANE REDUCTIONS IN OHIO SHALL BE CONCURRENT WITH THIS WORK. SEE SHEETS R148 AND R149 FOR DETAILS OF OHIO WORK.
- ALL PHASE 3 WORK ON I-471 NB FROM THE I-471 BRIDGE OVER 6TH STREET TO THE NORTH LIMITS OF THE PROJECT ON THE OHIO RIVER BRIDGE INCLUDING ALL BRIDGE WORK ON THE OHIO RIVER BRIDGE AND ALL WORK ON ENTRANCE RAMP A FROM KY 8 AND ALL WORK ON THE EXIT RAMP L TO KY 8. ALSO, ENOUGH OF THE WORK ON THE I-471 BRIDGE OVER 6TH STREET MUST BE COMPLETED, WITHOUT CLOSING MP RAMP M, SO THAT THE KY 8 RAMP L CAN BE REOPENED AT THE END OF THE 16 DAY PERIOD. THE REMAINING BRIDGE WORK ON THE I-471 BRIDGE OVER 6TH STREET IS TO BE COMPLETED WHEN THE MP RAMP M IS CLOSED, AND WITHOUT CLOSING KY 8 RAMP L DURING THAT TIME.
- ALL PHASE 3 WORK ON I-471 SB FROM THE I-471 BRIDGE OVER 6TH STREET TO THE NORTH LIMITS OF THE PROJECT ON THE OHIO RIVER BRIDGE INCLUDING ALL BRIDGE WORK ON THE OHIO RIVER BRIDGE, ALL WORK ON EXIT RAMP B TO KY 8 AND ALL WORK ON ENTRANCE RAMP K FROM KY 8. ALSO, ENOUGH OF THE WORK ON THE I-471 BRIDGE OVER 6TH STREET MUST BE COMPLETED, WITHOUT CLOSING MP RAMP N, SO THAT KY 8 RAMP K CAN BE REOPENED AT THE END OF THE 16 DAY PERIOD. THE REMAINING BRIDGE WORK ON THE I-471 BRIDGE OVER 6TH STREET IS TO BE COMPLETED WHEN MP RAMP N IS CLOSED, AND WITHOUT CLOSING KY 8 RAMP K AGAIN. RAMP CLOSURES/LANE REDUCTIONS IN OHIO SHALL BE CONCURRENT WITH THIS WORK. SEE SHEETS R148 AND R149 FOR DETAILS OF OHIO WORK.
- ALL PHASE 3 WORK ON I-471 NB BETWEEN GRAND AVENUE ENTRANCE RAMP K AND MEMORIAL PARKWAY EXIT RAMP D INCLUDING ALL WORK ON THE RAMPS AND THEIR GORE AREAS AND ALL WORK ON THE I-471 NB BRIDGE OVER CHESAPEAKE AVENUE.
- ALL PHASE 3 WORK ON I-471 SB BETWEEN GRAND AVENUE EXIT RAMP H AND MEMORIAL PARKWAY ENTRANCE RAMP A INCLUDING ALL WORK ON THE RAMPS AND THEIR GORE AREAS AND ALL WORK ON THE I-471 SB BRIDGE OVER CHESAPEAKE AVENUE.

THE START OF EACH 16 DAY PERIOD WILL BEGIN WHEN THE EXISTING NUMBER OF TRAFFIC LANES IN THESE AREAS IS REDUCED AND WILL END WHEN THE EXISTING NUMBER OF TRAFFIC LANES HAS BEEN RESTORED WITH THE EXCEPTION OF LANE TAPERS TO REDUCE THE NUMBER OF LANES ENTERING ADJACENT CONSTRUCTION AREAS.

ITEM e. ABOVE MUST BE PERFORMED IMMEDIATELY FOLLOWING THE COMPLETION OF ITEM c. ABOVE FOR A TOTAL COMPLETION TIME OF 32 CONSECUTIVE DAYS. SIMILARLY, ITEM f. ABOVE MUST BE PERFORMED IMMEDIATELY FOLLOWING THE COMPLETION OF ITEM d. ABOVE FOR A TOTAL COMPLETION TIME OF 32 CONSECUTIVE DAYS.

PRIOR TO BEGINNING WORK ON, OR AFTER COMPLETION OF, THE ACCELERATED WORK LISTED ABOVE ON I-471 NB OR SB FROM THE I-471 TWIN BRIDGES OVER 6TH STREET TO THE NORTH LIMITS OF THE PROJECT ON THE OHIO RIVER BRIDGE, TRAFFIC ON KY 8 RAMPS A OR B MAY NOT BE IMPEDED, OTHER THAN BY SHORT TERM SHOULDER CLOSURES APPROVED BY THE ENGINEER. NO LANE CLOSURES WILL BE PERMITTED IN OHIO EXCEPT FOR THE 32 DAYS REQUIRED FOR THIS ACCELERATED WORK IN EACH DIRECTION OF I-471.

PHASE 1 CONSTRUCTION

THE EXISTING NUMBER OF TRAFFIC LANES ARE TO BE MAINTAINED DURING PHASE 1 CONSTRUCTION FROM THE NORTH END OF THE I-471 NB/SB BIFURCATION (THE BEGINNING OF THE 16' PLANTER MEDIAN) TO THE NORTH END OF THE PROJECT. IN THE EVENT THE INSIDE SHOULDER PAVEMENT FAILS DURING PHASE 1 CONSTRUCTION, WITH THE APPROVAL OF THE ENGINEER, THE CONTRACTOR IS TO REDUCE TRAFFIC TO TWO LANES BY USING TRAFFIC BARRELS TO CLOSE THE INSIDE SHOULDER. THE CONTRACTOR WILL HAVE FIVE (5) WEEKDAY DAYS TO REPAIR THE FAILED AREAS AND RE-OPEN THE SHOULDER TO TRAFFIC. ALL ADDITIONAL WEEKDAY DAYS BEYOND THE FIVE DAY CLOSURE OF THE SHOULDER THAT THE SHOULDER REMAINS CLOSED WILL BE COUNTED AGAINST THE NUMBER OF DAYS THE CONTRACTOR SELECTED FOR TWO LANE REDUCTIONS IN PART B OF THE CONTRACT BID.

PHASE 1 CONSTRUCTION (CONTINUED)

ANY FAILURES OF THE INSIDE SHOULDER PAVEMENT DURING PHASE 1 WILL BE REPAIRED BY REMOVING THE EXISTING NINE INCH CONCRETE PAVEMENT AND INSTALLING TWO FOUR AND ONE-HALF INCH COURSES OF ITEM NO. 214 CL3 ASPHALT BASE 1.00D PG64-22 IN THE REMOVAL AREA. PAYMENT FOR THIS WORK WILL BE BY THE SQUARE YARDS OF ITEM NO. 2058 REMOVE PCC PAVEMENT AND TONS OF ASPHALT BASE INSTALLED AT THE UNIT PRICE BID FOR THESE ITEMS. REMOVAL OF THIS PAVEMENT IN PHASE 3 WILL BE PAID FOR BY BID ITEM 2091 REMOVE PAVEMENT AT THE UNIT PRICE PER SQUARE YARD BID FOR THIS ITEM. THE ENGINEER WILL DETERMINE WHEN THIS REPAIR IS NEEDED AND THE EXTENTS OF THE AREA TO BE REPAIRED. THIS WILL NOT RELIEVE THE CONTRACTOR OF ANY RESPONSIBILITY FOR WORK REQUIRED BY THE "MAINTENANCE OF EXISTING PAVEMENT" NOTE SHOWN ON GENERAL NOTES SHEET R201 FOR ANY OTHER PAVEMENT.

NORTHBOUND I-471 FROM THE I-471/US 27 INTERSECTION TO THE NORTH END OF THE I-471 NB/SB BIFURCATION, REDUCE TRAFFIC TO THE NUMBER OF LANES SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS, SHIFT TRAFFIC ONTO THE INSIDE SHOULDER AND DRIVING LANE, CONSTRUCT THE PHASE 1 SLIP RAMPS OR TAPERS TO AND FROM RAMPS AND INSTALL THE TEMPORARY BARRIER WALL. SOUTHBOUND I-471 FROM THE I-471/US 27 INTERSECTION TO THE NORTH END OF THE I-471 NB/SB BIFURCATION, REDUCE TRAFFIC TO THE NUMBER OF LANES SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS, SHIFT TRAFFIC ONTO THE INSIDE SHOULDER AND DRIVING LANE, CONSTRUCT THE PHASE 1 SLIP RAMPS OR TAPERS TO AND FROM RAMPS AND INSTALL THE TEMPORARY BARRIER WALL. FROM THE NORTH END OF THE I-471 NB/SB BIFURCATION TO THE NORTH END OF THE PROJECT, SHIFT THE I-471 TRAFFIC TO THE INSIDE SHOULDER, LANE, AND CENTER LANE, CONSTRUCT THE PHASE 1 SLIP RAMPS OR TAPERS TO AND FROM RAMPS AND INSTALL THE TEMPORARY BARRIER WALL AS SHOWN ON THE PHASE 1 MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS.

REMOVE THE EXISTING OUTSIDE SHOULDER WHERE SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS FOR PHASE 1 AND REPLACE WITH THE PERMANENT SHOULDER OR TEMPORARY SHOULDER (WHERE TEMPORARY SHOULDER PAVEMENT IS NOTED TO BE USED ON THE PLANS). CONSTRUCT THE TEMPORARY SHOULDER WIDENING FROM I-471 STA 18+50 TO I-471 SB STA 28+30. REMOVE THE EXISTING GUARDRAIL AND INSTALL NEW GUARDRAIL AS SHOWN ON THE PROJECT PLANS IN THE AREAS WHERE THE SHOULDERS ARE BEING REPLACED (PERMANENT OR TEMPORARY SHOULDER).

IF THE JPC PAVEMENT ALTERNATE IS CHOSEN, WHERE ALTERNATE PAVEMENT TYPES ARE INCLUDED IN THE PLANS, THE CONTRACTOR HAS THE OPTION OF USING EITHER JPC OR ASPHALT PAVEMENT FOR THE OUTSIDE SHOULDERS REPLACED IN PHASE 1 (SEE THE ROADWAY TYPICAL SECTIONS). ALL I-471 OUTSIDE SHOULDERS ARE TO BE REPLACED USING THE SAME PAVEMENT TYPE, INCLUDING THOSE NOT REPLACED UNTIL PHASE 3, IN THE AREAS WHERE ALTERNATE PAVEMENT TYPES ARE INCLUDED IN THE PLANS.

IN ORDER TO EXPEDITE THE OUTSIDE SHOULDER CONSTRUCTION IN PHASE 1, THE CONTRACTOR MAY ELECT TO USE TRAFFIC DRUMS INSTEAD OF THE TEMPORARY BARRIER WALL SHOWN IN THE PLANS AND THE MOT TYPICALS FOR PHASE 1. THIS OPTION WILL NOT BE ALLOWED FOR PHASE 2 OR 3. THE CONTRACTOR ALSO HAS THE OPTION TO PERFORM THIS WORK IN SEGMENTS IN ORDER TO MINIMIZE THE LENGTH OF THE CONSTRUCTION AREA THAT IS EXPOSED AT ANY ONE TIME.

PHASE 2 CONSTRUCTION

NORTHBOUND I-471 FROM THE BEGINNING OF THE PROJECT AT THE I-471/US 27 INTERSECTION TO THE END OF THE BARRIER WALL (JUST NORTH OF THE BEGINNING OF THE I-471 NB/SB BIFURCATION), REDUCE TRAFFIC TO ONE LANE AND SHIFT ONTO THE OUTSIDE SHOULDER AND INSTALL THE TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 2. CONSTRUCT THE INSIDE SHOULDER, INSIDE LANE AND HALF OF THE OUTSIDE LANE.

NORTHBOUND I-471 FROM THE END OF THE BARRIER WALL JUST NORTH OF THE I-471 NB/SB BIFURCATION TO RAMP B AT THE I-471/I-275 INTERCHANGE, REDUCE TRAFFIC TO ONE LANE, SHIFT TRAFFIC TO THE OUTSIDE, CONSTRUCT THE SLIP RAMPS TO THE I-275 RAMPS E AND B AND INSTALL THE TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 2. CONSTRUCT THE INSIDE LANE AND SHOULDER IN THE AVAILABLE AREAS WHILE MAINTAINING TRAFFIC TO THE I-275 RAMPS. CLOSE RAMPS E AND B AND CONSTRUCT THE REMAINING PHASE 2 WORK IN THIS AREA. SEE THE SECTION TITLED "RAMP CLOSURES" IN THESE NOTES FOR RAMP CLOSURE DETAILS.

SOUTHBOUND I-471 FROM THE BEGINNING OF THE PROJECT AT THE I-471/US 27 INTERSECTION TO JUST NORTH OF THE BEGINNING OF THE I-471 NB/SB BIFURCATION, REDUCE TRAFFIC TO TWO LANES AND SHIFT ONTO THE TEMPORARY WIDENED OUTSIDE SHOULDER AND PART OF THE DRIVING LANE AND RELOCATE THE PHASE 1 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS. CONSTRUCT THE INSIDE SHOULDER, INSIDE LANE AND CENTER LANE.

SOUTHBOUND I-471 FROM JUST NORTH OF THE I-471 NB/SB BIFURCATION TO I-275 RAMP C, REDUCE TRAFFIC TO TWO LANES (WITH TRAFFIC FROM RAMP C BECOMING THE SECOND LANE), SHIFT TRAFFIC TO THE OUTSIDE, CONSTRUCT THE SLIP RAMP FROM THE I-275 RAMP D AND RELOCATE THE PHASE 1 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS. CONSTRUCT THE INSIDE SHOULDER, INSIDE LANE AND HALF OF THE CENTER LANE IN THE AVAILABLE AREAS WHILE MAINTAINING TRAFFIC ON RAMP D. CLOSE RAMP D AND CONSTRUCT THE REMAINING PHASE 2 WORK IN THIS AREA. SEE THE SECTION TITLES "RAMP CLOSURES" IN THESE NOTES FOR RAMP CLOSURE DETAILS.

SOUTHBOUND I-471 FROM I-275 RAMP C TO I-275 RAMP F, REDUCE TRAFFIC TO ONE LANE, SHIFT TO THE OUTSIDE AND RELOCATE THE PHASE 1 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 2. CONSTRUCT THE INSIDE SHOULDER AND INSIDE LANE.

SOUTHBOUND I-471 FROM I-275 RAMP F TO I-275 RAMP A, REDUCE TRAFFIC TO TWO LANES, SHIFT TRAFFIC ONTO THE OUTSIDE SHOULDER AND DRIVING LANE, CONSTRUCT THE SLIP RAMP TO RAMP F AND RELOCATE THE PHASE 1 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 2. CONSTRUCT THE INSIDE SHOULDER AND INSIDE LANE, WIDENING TO THE INSIDE AS SHOWN IN THE PROJECT PLANS, IN THE AVAILABLE AREAS WHILE MAINTAINING TRAFFIC ON RAMP F.

NORTHBOUND I-471 FROM I-275 RAMP B AND SOUTHBOUND I-471 FROM THE I-275 RAMP A, TO THE END OF THE PROJECT, REDUCE TRAFFIC TO TWO LANES, SHIFT TRAFFIC TO THE OUTSIDE, CONSTRUCT THE SLIP RAMPS OR TAPERS TO AND FROM THE RAMPS AT THE US 27, GRAND AVENUE, MEMORIAL PARKWAY AND KY 8 INTERCHANGES AND RELOCATE THE PHASE 1 TEMPORARY BARRIER WALL, OR INSTALL NEW TEMPORARY BARRIER WALL, AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 2. CONSTRUCT THE INSIDE SHOULDER, INSIDE LANE AND HALF OF THE CENTER LANE. REMOVE APPROXIMATELY 1480' OF THE I-471 SOUTHBOUND INSIDE MEDIAN BARRIER, AND MATERIAL WITHIN THE 16' PLANTER MEDIAN, STARTING AT THE SOUTHERN END OF THE BARRIER. CONSTRUCT THE WIDENED SECTION OF I-471 SOUTHBOUND AND NEW CONCRETE MEDIAN BARRIER AS SHOWN IN THE PROJECT PLANS. PERFORM THE BRIDGE WORK SHOWN IN THE BRIDGE PROPOSAL IN THE AVAILABLE AREAS ON THE I-471 BRIDGES OVER US 27, GRAND AVENUE, CHESAPEAKE AVENUE, 6TH STREET, KY 8 AND THE OHIO RIVER.

MAINTENANCE OF TRAFFIC NOTES

FILE NAME: P:\CIVIL\1471\6-2021\00\CONTRACT PLANS AND PROPOSAL REVISIONS AFTER SUBMITTAL\PLANS REVISION 3\14714500MT.DGN

USER: jrcopier
DATE PLOTTED: February 10, 2012

E-SHEET NAME:

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COUNTY OF	ITEM NO.	SHEET NO.
CAMPBELL	6-2021.00	R145

TRAFFIC CONTROL GENERAL

EXCEPT AS PROVIDED HEREIN, MAINTAIN AND CONTROL TRAFFIC IN ACCORDANCE WITH THE 2008 STANDARD SPECIFICATIONS, THE STANDARD DRAWINGS AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), CURRENT EDITIONS. EXCEPT FOR THE ROADWAY AND TRAFFIC CONTROL BID ITEMS LISTED, ALL ITEMS OF WORK NECESSARY TO MAINTAIN AND CONTROL TRAFFIC WILL BE PAID AT THE LUMP SUM BID PRICE TO "MAINTAIN AND CONTROL TRAFFIC". ALL LANE CLOSURES USED ON THE PROJECT WILL BE IN COMPLIANCE WITH THE APPROPRIATE STANDARD DRAWINGS.

CONTRARY TO SECTION 106.01, TRAFFIC CONTROL DEVICES USED ON THIS PROJECT MAY BE NEW, OR USED IN LIKE NEW CONDITION, AT THE BEGINNING OF THE WORK AND MAINTAINED IN LIKE NEW CONDITION UNTIL COMPLETION OF THE WORK. TRAFFIC CONTROL DEVICES WILL CONFORM TO CURRENT MUTCD SPECIFICATIONS.

DURING CONSTRUCTION, SIGNS AND PORTABLE CHANGEABLE MESSAGE SIGNS WILL BE INSTALLED AT LOCATIONS SPECIFIED BY THE ENGINEER. SEE THE MAINTENANCE OF TRAFFIC CONSTRUCTION PHASING PLAN FOR TRAFFIC CONTROL DETAILS DURING EACH PHASE OF CONSTRUCTION. THE MINIMUM LANE WIDTH FOR THIS PROJECT SHALL BE 11 FEET. DURING CONSTRUCTION, THE EXISTING POSTED 65 MPH SPEED LIMIT SHALL BE REDUCED TO 55 MPH.

IN THESE NOTES, A WORK WEEK SHALL BE FROM 6 AM ON MONDAY TO 8 PM ON FRIDAY AND A WEEKEND SHALL BE FROM 8 PM ON FRIDAY TO 6 AM THE FOLLOWING MONDAY.

MAINTENANCE OF TRAFFIC ACTIVITIES WILL BE REQUIRED ON THE OHIO SIDE OF THE TWIN I-471 BRIDGES OVER THE OHIO RIVER AT THE NORTH END OF THIS PROJECT.

DRUMS OR TEMPORARY CONCRETE BARRIER WALL ARE REQUIRED FOR ALL TRAFFIC OPERATIONS IN KENTUCKY AND OHIO. THE USE OF CONES WILL NOT BE PERMITTED.

NO PAYMENT WILL BE AWARDED FOR VALUE ENGINEERING THE MAINTENANCE OF TRAFFIC PLANS.

CONSTRUCTION PHASING AND SEQUENCE OF CONSTRUCTION

CONSTRUCTION PHASING AND THE SEQUENCE OF CONSTRUCTION WILL BE MAINTAINED AS SHOWN IN THE PLANS AND THE MAINTENANCE OF TRAFFIC NOTES UNLESS OTHERWISE APPROVED BY THE ENGINEER.

ALL I-471 NORTHBOUND WORK AND ASSOCIATED RAMP WORK AT THE I-275, US 27, GRAND AVENUE, MEMORIAL PARKWAY AND KY 8 INTERCHANGES IS TO BE COMPLETED BY DECEMBER 1, 2012. NO TRAFFIC IMPACTS WILL BE PERMITTED ON I-471 SOUTHBOUND OR ASSOCIATED RAMPS PRIOR TO COMPLETION OF ALL I-471 NORTHBOUND WORK AND ASSOCIATED RAMPS.

NO TRAFFIC IMPACTS WILL BE ALLOWED ON I-471, AND ASSOCIATED RAMPS, IN EITHER DIRECTION FROM DECEMBER 1, 2012 UNTIL APRIL 1, 2013.

CONSTRUCTION SIGNING IS NOT TO BE INSTALLED ON I-471 SOUTHBOUND PRIOR TO BEGINNING WORK ON THE SOUTHBOUND SIDE. SIGNS FOR TEMPORARY SHOULDER CLOSURES ON I-471 SOUTHBOUND ARE TO BE INSTALLED, THEN REMOVED WHEN THE SHOULDER IS REOPENED.

IF THE ASPHALT PAVEMENT ALTERNATE IS CHOSEN, THE ASPHALT SURFACE COURSE IS NOT TO BE PLACED UNTIL ALL AREAS, OF THE DIRECTION UNDER CONSTRUCTION, HAVE THEIR FINAL BASE COURSE. FINAL STRIPING IS THEN TO BE PLACED ON THE SURFACE COURSE.

ALL I-471 SOUTHBOUND WORK AND ASSOCIATED RAMP WORK AT THE I-275, US 27, GRAND AVENUE, MEMORIAL PARKWAY AND KY 8 INTERCHANGES IS TO BE COMPLETED BY DECEMBER 1, 2013.

WORK ON I-471 NB AND SB FROM THE BEGINNING OF THE PROJECT AT STA 15+09.42 (US 27) TO I-471 NB STA 53+00 AND I-471 SB STA 49+00 IS ONLY TO BE DONE DURING THE SUMMER MONTHS WHEN NORTHERN KENTUCKY UNIVERSITY IS NOT IN A SPRING OR FALL SEMESTER. FOR I-471 NB, WORK MAY NOT BEGIN PRIOR TO MAY 7, 2012 AND MUST BE COMPLETED BY AUGUST 15, 2012. FOR I-471 SB, WORK MAY NOT BEGIN UNTIL AFTER THE 2013 SPRING COMMENCEMENT CEREMONY AND MUST BE COMPLETED 10 DAYS PRIOR TO START OF FALL CLASSES IN 2013 (SPECIFIC DATES TO BE DETERMINED). THE CONTRACTOR IS TO NOTIFY THE ENGINEER AT LEAST 14 DAYS PRIOR TO BEGINNING WORK ON EITHER I-471 NB OR SB.

THE CONTRACTOR MUST NOTIFY THE ENGINEER 14 DAYS PRIOR TO MAJOR TRAFFIC CHANGES OR RAMP CLOSURES. SEE THE SPECIAL NOTE FOR FIXED COMPLETION DATE AND LIQUIDATED DAMAGES FOR ADDITIONAL DETAILS.

RAMP CLOSURES

ONLY ONE RAMP MAY BE CLOSED AT A TIME UNLESS SPECIFICALLY NOTED OTHERWISE IN THESE PLANS OR APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL PLACE A PORTABLE CHANGEABLE MESSAGE SIGN WARNING OF PENDING RAMP CLOSURE IN THE AREA OF EACH RAMP TO BE CLOSED AT LEAST 10 DAYS PRIOR TO ITS CLOSURE.

THE CONTRACTOR WILL BE PERMITTED TO CLOSE RAMPS B, C, D, E AND G AT THE I-471/I-275 INTERCHANGE FOR A PERIOD OF 9 CONSECUTIVE DAYS (TWO WEEKENDS AND ONE WORK WEEK) AND ALL RAMP WORK IS TO BE COMPLETED DURING THAT TIME. THE CONTRACTOR WILL BE PERMITTED TO CLOSE RAMP A AT THE I-275/I-471 INTERCHANGE FOR A PERIOD OF 16 CONSECUTIVE DAYS (THREE WEEKENDS AND TWO WORK WEEKS) AND ALL RAMP WORK IS TO BE COMPLETED BY THE END OF THE PERIOD INCLUDING THE SEALING OF SHOULDERS. THE CLOSURE OF RAMPS E AND G MUST OCCUR DURING THE ACCELERATED CONSTRUCTION OF THE SECTION OF I-471 NB THEY JOIN AS SHOWN IN THE NOTE TITLED "ACCELERATED CONSTRUCTION".

THE TWO LANE RAMPS AT THE I-471/I-275 INTERCHANGE, RAMPS F AND H, ARE NOT TO BE CLOSED. TRAFFIC IS TO BE REDUCED TO ONE LANE AND SHIFTED PARTIALLY ONTO THE SHOULDER WHILE WORK IS BEING PERFORMED ON HALF OF THE RAMP, THEN SHIFTED TO THE OPPOSITE SIDE TO PERFORM THE REMAINING WORK ON THE RAMP. SEE THE MAINTENANCE OF TRAFFIC PLANS, TYPICAL SECTIONS AND NOTES FOR DETAILS ON CONSTRUCTION PHASING FOR THESE RAMPS.

THE CONTRACTOR WILL BE PERMITTED TO CLOSE ALL FOUR US 27 RAMPS FOR A PERIOD OF 16 CONSECUTIVE DAYS (THREE WEEKENDS AND TWO WORK WEEKS) AND ALL RAMP WORK IS TO BE COMPLETED DURING THAT TIME INCLUDING THE SEALING OF SHOULDERS.

THE CONTRACTOR WILL BE PERMITTED TO CLOSE GRAND AVENUE (GA) RAMPS E, EE, AND F FOR A PERIOD OF 16 CONSECUTIVE DAYS (THREE WEEKENDS AND TWO WORK WEEKS) AND ALL RAMP WORK IS TO BE COMPLETED DURING THAT TIME.

THE CONTRACTOR WILL BE PERMITTED TO CLOSE MEMORIAL PARKWAY (MP) RAMPS M AND N FOR A PERIOD OF 16 CONSECUTIVE DAYS (THREE WEEKENDS AND TWO WORK WEEKS) AND ALL RAMP WORK IS TO BE COMPLETED DURING THAT TIME.

RAMP CLOSURES (CONTINUED)

THE CONTRACTOR WILL BE PERMITTED TO CLOSE GA RAMP K AND MP RAMP D ON I-471 NORTHBOUND SIMULTANEOUSLY FOR 16 DAYS (THREE WEEKENDS AND TWO WORK WEEKS) DURING PHASE 3. THE CONTRACTOR WILL ALSO BE PERMITTED TO CLOSE GA RAMP H AND MP RAMP A ON I-471 SOUTHBOUND SIMULTANEOUSLY FOR 16 DAYS (THREE WEEKENDS AND TWO WORK WEEKS) DURING PHASE 3. DURING THE 16 DAYS THE RAMPS ON ONE DIRECTION OF I-471 ARE CLOSED, ALL WORK ON THE MAINLINE IN PHASE 3, INCLUDING THE WORK ON THE BRIDGE OVER CHESAPEAKE AVENUE, MUST BE COMPLETED.

THE CONTRACTOR WILL BE PERMITTED TO CLOSE KY 8 RAMPS A, B, K AND L (RAMPS A AND L SIMULTANEOUSLY, RAMPS B AND K SIMULTANEOUSLY) FOR A PERIOD OF 16 CONSECUTIVE DAYS (THREE WEEKENDS AND TWO WORK WEEKS) IN PHASE 3 AND ALL RAMP WORK IS TO BE COMPLETED BY THE END OF THE PERIOD. THESE RAMPS SHALL BE CLOSED DURING THE SAME PERIOD AS THE ACCELERATED CONSTRUCTION SECTION OF I-471 FROM THE I-471 TWIN BRIDGES OVER 6TH STREET TO THE END OF THE PROJECT IS PERFORMED.

COMPLETION DATE DISINCENTIVE

SEE THE SPECIAL NOTE FOR FIXED COMPLETION DATES AND LIQUIDATED DAMAGES FOR DETAILS CONCERNING LIQUIDATED DAMAGES TO BE CHARGED TO THE CONTRACTOR FOR FAILURE TO COMPLETE ANY WORK BY THE SPECIFIED COMPLETION DATE.

ACCELERATED CONSTRUCTION

FOR EACH OF THE FOLLOWING ACCELERATED CONSTRUCTION AREAS THE CONTRACTOR WILL HAVE 16 CONSECUTIVE DAYS (THREE WEEKENDS AND TWO WORK WEEKS) TO COMPLETE ALL WORK, INCLUDING ANY BRIDGE WORK, SHOWN IN THE PLANS FOR THAT AREA (WITH THE EXCEPTION OF THE ASPHALT SURFACE COURSE WHEN ASPHALT IS CHOSEN AS THE ALTERNATE PAVEMENT TYPE OR WHERE NO ALTERNATE PAVEMENT IS SHOWN):

- a. ALL PHASE 2 WORK ON I-471 NB FROM US 27 TO I-275 NB STATION 53+00
- b. ALL PHASE 3 WORK ON I-471 NB FROM US 27 TO I-275 NB STATION 53+00
- c. ALL PHASE 2 WORK ON I-471 NB FROM THE SOUTH END OF THE I-471 BRIDGE OVER 6TH STREET TO THE NORTH LIMITS OF THE PROJECT ON THE OHIO RIVER BRIDGE INCLUDING ALL BRIDGE WORK ON BOTH BRIDGES.
- d. ALL PHASE 2 WORK ON I-471 SB FROM THE SOUTH END OF THE I-471 BRIDGE OVER 6TH STREET TO THE NORTH LIMITS OF THE PROJECT ON THE OHIO RIVER BRIDGE INCLUDING ALL BRIDGE WORK ON BOTH BRIDGES. RAMP CLOSURES/LANE REDUCTIONS IN OHIO SHALL BE CONCURRENT WITH THIS WORK. SEE SHEETS R148 AND R149 FOR DETAILS OF OHIO WORK.
- e. ALL PHASE 3 WORK ON I-471 NB FROM THE I-471 BRIDGE OVER 6TH STREET TO THE NORTH LIMITS OF THE PROJECT ON THE OHIO RIVER BRIDGE INCLUDING ALL BRIDGE WORK ON THE OHIO RIVER BRIDGE AND ALL WORK ON ENTRANCE RAMP A FROM KY 8 AND ALL WORK ON THE EXIT RAMP L TO KY 8. ALSO, ENOUGH OF THE WORK ON THE I-471 BRIDGE OVER 6TH STREET MUST BE COMPLETED, WITHOUT CLOSING MP RAMP M, SO THAT THE KY 8 RAMP L CAN BE REOPENED AT THE END OF THE 16 DAY PERIOD. THE REMAINING BRIDGE WORK ON THE I-471 BRIDGE OVER 6TH STREET IS TO BE COMPLETED WHEN THE MP RAMP M IS CLOSED, AND WITHOUT CLOSING KY 8 RAMP L DURING THAT TIME.
- f. ALL PHASE 3 WORK ON I-471 SB FROM THE I-471 BRIDGE OVER 6TH STREET TO THE NORTH LIMITS OF THE PROJECT ON THE OHIO RIVER BRIDGE INCLUDING ALL BRIDGE WORK ON THE OHIO RIVER BRIDGE, ALL WORK ON EXIT RAMP B TO KY 8 AND ALL WORK ON ENTRANCE RAMP K FROM KY 8. ALSO, ENOUGH OF THE WORK ON THE I-471 BRIDGE OVER 6TH STREET MUST BE COMPLETED, WITHOUT CLOSING MP RAMP N, SO THAT KY 8 RAMP K CAN BE REOPENED AT THE END OF THE 16 DAY PERIOD. THE REMAINING BRIDGE WORK ON THE I-471 BRIDGE OVER 6TH STREET IS TO BE COMPLETED WHEN MP RAMP N IS CLOSED, AND WITHOUT CLOSING KY 8 RAMP K AGAIN. RAMP CLOSURES/LANE REDUCTIONS IN OHIO SHALL BE CONCURRENT WITH THIS WORK. SEE SHEETS R148 AND R149 FOR DETAILS OF OHIO WORK.
- g. ALL PHASE 3 WORK ON I-471 NB BETWEEN GRAND AVENUE ENTRANCE RAMP K AND MEMORIAL PARKWAY EXIT RAMP D INCLUDING ALL WORK ON THE RAMPS AND THEIR GORE AREAS AND ALL WORK ON THE I-471 NB BRIDGE OVER CHESAPEAKE AVENUE.
- h. ALL PHASE 3 WORK ON I-471 SB BETWEEN GRAND AVENUE EXIT RAMP H AND MEMORIAL PARKWAY ENTRANCE RAMP A INCLUDING ALL WORK ON THE RAMPS AND THEIR GORE AREAS AND ALL WORK ON THE I-471 SB BRIDGE OVER CHESAPEAKE AVENUE.

THE START OF EACH 16 DAY PERIOD WILL BEGIN WHEN THE EXISTING NUMBER OF TRAFFIC LANES IN THESE AREAS IS REDUCED AND WILL END WHEN THE EXISTING NUMBER OF TRAFFIC LANES HAS BEEN RESTORED WITH THE EXCEPTION OF LANE TAPERS TO REDUCE THE NUMBER OF LANES ENTERING ADJACENT CONSTRUCTION AREAS.

ITEM e. ABOVE MUST BE PERFORMED IMMEDIATELY FOLLOWING THE COMPLETION OF ITEM c. ABOVE FOR A TOTAL COMPLETION TIME OF 32 CONSECUTIVE DAYS. SIMILARLY, ITEM f. ABOVE MUST BE PERFORMED IMMEDIATELY FOLLOWING THE COMPLETION OF ITEM d. ABOVE FOR A TOTAL COMPLETION TIME OF 32 CONSECUTIVE DAYS.

PRIOR TO BEGINNING WORK ON, OR AFTER COMPLETION OF, THE ACCELERATED WORK LISTED ABOVE ON I-471 NB OR SB FROM THE I-471 TWIN BRIDGES OVER 6TH STREET TO THE NORTH LIMITS OF THE PROJECT ON THE OHIO RIVER BRIDGE, TRAFFIC ON KY 8 RAMPS A OR B MAY NOT BE IMPEDED, OTHER THAN BY SHORT TERM SHOULDER CLOSURES APPROVED BY THE ENGINEER. NO LANE CLOSURES WILL BE PERMITTED IN OHIO EXCEPT FOR THE 32 DAYS REQUIRED FOR THIS ACCELERATED WORK IN EACH DIRECTION OF I-471.

PHASE 1 CONSTRUCTION

THE EXISTING NUMBER OF TRAFFIC LANES ARE TO BE MAINTAINED DURING PHASE 1 CONSTRUCTION FROM THE NORTH END OF THE I-471 NB/SB BIFURCATION (THE BEGINNING OF THE 16' PLANTER MEDIAN) TO THE NORTH END OF THE PROJECT. IN THE EVENT THE INSIDE SHOULDER PAVEMENT FAILS DURING PHASE 1 CONSTRUCTION, WITH THE APPROVAL OF THE ENGINEER, THE CONTRACTOR IS TO REDUCE TRAFFIC TO TWO LANES BY USING TRAFFIC BARRELS TO CLOSE THE INSIDE SHOULDER. THE CONTRACTOR WILL HAVE FIVE (5) WEEKDAY DAYS TO REPAIR THE FAILED AREAS AND RE-OPEN THE SHOULDER TO TRAFFIC. ALL ADDITIONAL WEEKDAY DAYS BEYOND THE FIVE DAY CLOSURE OF THE SHOULDER THAT THE SHOULDER REMAINS CLOSED WILL BE COUNTED AGAINST THE NUMBER OF DAYS THE CONTRACTOR SELECTED FOR TWO LANE REDUCTIONS IN PART B OF THE CONTRACT BID.

PHASE 1 CONSTRUCTION (CONTINUED)

ANY FAILURES OF THE INSIDE SHOULDER PAVEMENT DURING PHASE 1 WILL BE REPAIRED BY REMOVING THE EXISTING NINE INCH CONCRETE PAVEMENT AND INSTALLING TWO FOUR AND ONE-HALF INCH COURSES OF ITEM NO. 214 CL3 ASPHALT BASE 1.00D PG64-22 IN THE REMOVAL AREA. PAYMENT FOR THIS WORK WILL BE BY THE SQUARE YARDS OF ITEM NO. 2058 REMOVE PCC PAVEMENT AND TONS OF ASPHALT BASE INSTALLED AT THE UNIT PRICE BID FOR THESE ITEMS. REMOVAL OF THIS PAVEMENT IN PHASE 3 WILL BE PAID FOR BY BID ITEM 2091 REMOVE PAVEMENT AT THE UNIT PRICE PER SQUARE YARD BID FOR THIS ITEM. THE ENGINEER WILL DETERMINE WHEN THIS REPAIR IS NEEDED AND THE EXTENTS OF THE AREA TO BE REPAIRED. THIS WILL NOT RELIEVE THE CONTRACTOR OF ANY RESPONSIBILITY FOR WORK REQUIRED BY THE "MAINTENANCE OF EXISTING PAVEMENT" NOTE SHOWN ON GENERAL NOTES SHEET R201 FOR ANY OTHER PAVEMENT.

NORTHBOUND I-471 FROM THE I-471/US 27 INTERSECTION TO THE NORTH END OF THE I-471 NB/SB BIFURCATION, REDUCE TRAFFIC TO THE NUMBER OF LANES SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS, SHIFT TRAFFIC ONTO THE INSIDE SHOULDER AND DRIVING LANE, CONSTRUCT THE PHASE 1 SLIP RAMPS OR TAPERS TO AND FROM RAMPS AND INSTALL THE TEMPORARY BARRIER WALL. SOUTHBOUND I-471 FROM THE I-471/US 27 INTERSECTION TO THE NORTH END OF THE I-471 NB/SB BIFURCATION, REDUCE TRAFFIC TO THE NUMBER OF LANES SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS, SHIFT TRAFFIC ONTO THE INSIDE SHOULDER AND DRIVING LANE, CONSTRUCT THE PHASE 1 SLIP RAMPS OR TAPERS TO AND FROM RAMPS AND INSTALL THE TEMPORARY BARRIER WALL. FROM THE NORTH END OF THE I-471 NB/SB BIFURCATION TO THE NORTH END OF THE PROJECT, SHIFT THE I-471 TRAFFIC TO THE INSIDE SHOULDER, LANE, AND CENTER LANE, CONSTRUCT THE PHASE 1 SLIP RAMPS OR TAPERS TO AND FROM RAMPS AND INSTALL THE TEMPORARY BARRIER WALL AS SHOWN ON THE PHASE 1 MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS.

REMOVE THE EXISTING OUTSIDE SHOULDER WHERE SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS FOR PHASE 1 AND REPLACE WITH THE PERMANENT SHOULDER OR TEMPORARY SHOULDER (WHERE TEMPORARY SHOULDER PAVEMENT IS NOTED TO BE USED ON THE PLANS). CONSTRUCT THE TEMPORARY SHOULDER WIDENING FROM I-471 STA 18+50 TO I-471 SB STA 28+30. REMOVE THE EXISTING GUARDRAIL AND INSTALL NEW GUARDRAIL AS SHOWN ON THE PROJECT PLANS IN THE AREAS WHERE THE SHOULDERS ARE BEING REPLACED (PERMANENT OR TEMPORARY SHOULDER).

IF THE JPC PAVEMENT ALTERNATE IS CHOSEN, WHERE ALTERNATE PAVEMENT TYPES ARE INCLUDED IN THE PLANS, THE CONTRACTOR HAS THE OPTION OF USING EITHER JPC OR ASPHALT PAVEMENT FOR THE OUTSIDE SHOULDERS REPLACED IN PHASE 1 (SEE THE ROADWAY TYPICAL SECTIONS). ALL I-471 OUTSIDE SHOULDERS ARE TO BE REPLACED USING THE SAME PAVEMENT TYPE, INCLUDING THOSE NOT REPLACED UNTIL PHASE 3, IN THE AREAS WHERE ALTERNATE PAVEMENT TYPES ARE INCLUDED IN THE PLANS.

IN ORDER TO EXPEDITE THE OUTSIDE SHOULDER CONSTRUCTION IN PHASE 1, THE CONTRACTOR MAY ELECT TO USE TRAFFIC DRUMS INSTEAD OF THE TEMPORARY BARRIER WALL SHOWN IN THE PLANS AND THE MOT TYPICALS FOR PHASE 1. THIS OPTION WILL NOT BE ALLOWED FOR PHASE 2 OR 3. THE CONTRACTOR ALSO HAS THE OPTION TO PERFORM THIS WORK IN SEGMENT'S IN ORDER TO MINIMIZE THE LENGTH OF THE CONSTRUCTION AREA THAT IS EXPOSED AT ANY ONE TIME.

PHASE 2 CONSTRUCTION

NORTHBOUND I-471 FROM THE BEGINNING OF THE PROJECT AT THE I-471/US 27 INTERSECTION TO THE END OF THE BARRIER WALL (JUST NORTH OF THE BEGINNING OF THE I-471 NB/SB BIFURCATION), REDUCE TRAFFIC TO ONE LANE AND SHIFT ONTO THE OUTSIDE SHOULDER AND INSTALL THE TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 2. CONSTRUCT THE INSIDE SHOULDER, INSIDE LANE AND HALF OF THE OUTSIDE LANE.

NORTHBOUND I-471 FROM THE END OF THE BARRIER WALL JUST NORTH OF THE I-471 NB/SB BIFURCATION TO RAMP B AT THE I-471/I-275 INTERCHANGE, REDUCE TRAFFIC TO ONE LANE, SHIFT TRAFFIC TO THE OUTSIDE, CONSTRUCT THE SLIP RAMPS TO THE I-275 RAMPS E AND B AND INSTALL THE TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 2. CONSTRUCT THE INSIDE LANE AND SHOULDER IN THE AVAILABLE AREAS WHILE MAINTAINING TRAFFIC TO THE I-275 RAMPS. CLOSE RAMPS E AND B AND CONSTRUCT THE REMAINING PHASE 2 WORK IN THIS AREA. SEE THE SECTION TITLED "RAMP CLOSURES" IN THESE NOTES FOR RAMP CLOSURE DETAILS.

SOUTHBOUND I-471 FROM THE BEGINNING OF THE PROJECT AT THE I-471/US 27 INTERSECTION TO JUST NORTH OF THE BEGINNING OF THE I-471 NB/SB BIFURCATION, REDUCE TRAFFIC TO TWO LANES AND SHIFT ONTO THE TEMPORARY WIDENED OUTSIDE SHOULDER AND PART OF THE DRIVING LANE AND RELOCATE THE PHASE 1 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS. CONSTRUCT THE INSIDE SHOULDER, INSIDE LANE AND CENTER LANE.

SOUTHBOUND I-471 FROM JUST NORTH OF THE I-471 NB/SB BIFURCATION TO I-275 RAMP C, REDUCE TRAFFIC TO TWO LANES (WITH TRAFFIC FROM RAMP C BECOMING THE SECOND LANE), SHIFT TRAFFIC TO THE OUTSIDE, CONSTRUCT THE SLIP RAMP FROM THE I-275 RAMP D AND RELOCATE THE PHASE 1 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS. CONSTRUCT THE INSIDE SHOULDER, INSIDE LANE AND HALF OF THE CENTER LANE IN THE AVAILABLE AREAS WHILE MAINTAINING TRAFFIC ON RAMP D. CLOSE RAMP D AND CONSTRUCT THE REMAINING PHASE 2 WORK IN THIS AREA. SEE THE SECTION TITLES "RAMP CLOSURES" IN THESE NOTES FOR RAMP CLOSURE DETAILS.

SOUTHBOUND I-471 FROM I-275 RAMP C TO I-275 RAMP F, REDUCE TRAFFIC TO ONE LANE, SHIFT TO THE OUTSIDE AND RELOCATE THE PHASE 1 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 2. CONSTRUCT THE INSIDE SHOULDER AND INSIDE LANE.

SOUTHBOUND I-471 FROM I-275 RAMP F TO I-275 RAMP A, REDUCE TRAFFIC TO TWO LANES, SHIFT TRAFFIC ONTO THE OUTSIDE SHOULDER AND DRIVING LANE, CONSTRUCT THE SLIP RAMP TO RAMP F AND RELOCATE THE PHASE 1 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 2. CONSTRUCT THE INSIDE SHOULDER AND INSIDE LANE, WIDENING TO THE INSIDE AS SHOWN IN THE PROJECT PLANS, IN THE AVAILABLE AREAS WHILE MAINTAINING TRAFFIC ON RAMP F.

NORTHBOUND I-471 FROM I-275 RAMP B AND SOUTHBOUND I-471 FROM THE I-275 RAMP A, TO THE END OF THE PROJECT, REDUCE TRAFFIC TO TWO LANES, SHIFT TRAFFIC TO THE OUTSIDE, CONSTRUCT THE SLIP RAMPS OR TAPERS TO AND FROM THE RAMPS AT THE US 27, GRAND AVENUE, MEMORIAL PARKWAY AND KY 8 INTERCHANGES AND RELOCATE THE PHASE 1 TEMPORARY BARRIER WALL, OR INSTALL NEW TEMPORARY BARRIER WALL, AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 2. CONSTRUCT THE INSIDE SHOULDER, INSIDE LANE AND HALF OF THE CENTER LANE. REMOVE APPROXIMATELY 1480' OF THE I-471 SOUTHBOUND INSIDE MEDIAN BARRIER, AND MATERIAL WITHIN THE 16' PLANTER MEDIAN, STARTING AT THE SOUTHERN END OF THE BARRIER. CONSTRUCT THE WIDENED SECTION OF I-471 SOUTHBOUND AND NEW CONCRETE MEDIAN BARRIER AS SHOWN IN THE PROJECT PLANS. PERFORM THE BRIDGE WORK SHOWN IN THE BRIDGE PROPOSAL IN THE AVAILABLE AREAS ON THE I-471 BRIDGES OVER US 27, GRAND AVENUE, CHESAPEAKE AVENUE, 6TH STREET, KY 8 AND THE OHIO RIVER.

MAINTENANCE OF TRAFFIC NOTES

FILE NAME: P:\CIVIL\1471\6-2021.00\CONTRACT PLANS AND PROPOSAL REVISIONS AFTER SUBMITTAL\PLANS REVISION 3\RI45000MT.DGN

USER: jrcopler DATE PLOTTED: February 10, 2012

E-SHEET NAME:

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COUNTY OF	ITEM NO.	SHEET NO.
CAMPBELL	6-2021.00	R146

FILE NAME: P:\CIVIL\1471\6-2021.00\CONTRACT PLANS AND PROPOSAL REVISIONS AFTER SUBMITTAL\PLANS REVISION 3\14600MT.DGN
 USER: fropier
 DATE PLOTTED: February 10, 2012
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PHASE 2A CONSTRUCTION

CONSTRUCT THE TEMPORARY I-275 RAMP F EXIT TAPER AND LANE FROM I-471 SOUTHBOUND ON THE NEWLY CONSTRUCTED INSIDE SHOULDER, INSTALL THE TEMPORARY BARRIER WALL ON THE TEMPORARY RAMP F EXIT LANE AND SHIFT RAMP F TRAFFIC ONTO THE EXIT LANE. REPOSITION THE TEMPORARY BARRIER WALL ON I-471 SOUTHBOUND INTO THE NEW LOCATION AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS FOR PHASE 2A. CONSTRUCT THE REMAINING PHASE 2 WORK IN THE AREA.

PHASE 3 CONSTRUCTION

NORTHBOUND I-471 FROM THE BEGINNING OF THE PROJECT AT THE I-471/US 27 INTERSECTION TO THE END OF THE BARRIER WALL (JUST NORTH OF THE BEGINNING OF THE I-471 NB/SB BIFURCATION), REDUCE TRAFFIC TO ONE LANE AND SHIFT ONTO THE INSIDE LANE AND SHOULDER, RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 3. CONSTRUCT THE OUTSIDE SHOULDER AND HALF OF THE OUTSIDE LANE.

NORTHBOUND I-471 FROM THE END OF THE BARRIER WALL JUST NORTH OF THE I-471 NB/SB BIFURCATION TO RAMP B AT THE I-471/I-275 INTERCHANGE, REDUCE TRAFFIC TO ONE LANE, SHIFT TRAFFIC TO THE INSIDE, CONSTRUCT THE SLIP RAMP TO I-275 RAMP H, THE TAPER TO I-275 RAMP G AND RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 3. CONSTRUCT THE OUTSIDE LANE AND SHOULDER IN THE AVAILABLE AREAS WHILE MAINTAINING TRAFFIC TO RAMP G AND H. CLOSE RAMP G AND CONSTRUCT THE REMAINING PHASE 3 WORK IN THE RAMP AREA. SEE THE SECTION TITLED "RAMP CLOSURES" IN THESE NOTES FOR RAMP CLOSURE DETAILS.

SOUTHBOUND I-471 FROM THE BEGINNING OF THE PROJECT AT THE I-471/US 27 INTERSECTION TO JUST NORTH OF THE BEGINNING OF THE I-471 NB/SB BIFURCATION, REDUCE TRAFFIC TO TWO LANES AND SHIFT TO INSIDE LANE AND SHOULDER AND RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 3. CONSTRUCT THE OUTSIDE LANE AND ANY PERMANENT OUTSIDE SHOULDER NOT CONSTRUCTED IN PHASE 1. REMOVE THE TEMPORARY SHOULDER WIDENING AND INSTALL GUARDRAIL AT THE SAME LOCATION IT WAS PRIOR TO REMOVAL IN PHASE 1.

SOUTHBOUND I-471 FROM JUST NORTH OF THE I-471 NB/SB BIFURCATION TO I-275 RAMP C, REDUCE TRAFFIC TO TWO LANES (WITH TRAFFIC FROM RAMP C BECOMING THE SECOND LANE), SHIFT TRAFFIC TO THE INSIDE, CONSTRUCT THE SLIP RAMP FROM RAMP C AND THE I-275 RAMP D TAPER TO I-471 SB AND RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS. CONSTRUCT THE OUTSIDE LANE, HALF OF THE CENTER LANE AND ANY PART OF THE PERMANENT OUTSIDE SHOULDER NOT CONSTRUCTED IN PHASE 1 IN THE AVAILABLE AREAS WHILE MAINTAINING TRAFFIC ON RAMP C. CLOSE RAMP C AND CONSTRUCT THE REMAINING PHASE 3 WORK IN THE AREA. SEE THE SECTION TITLED "RAMP CLOSURES" IN THESE NOTES FOR RAMP CLOSURE DETAILS.

SOUTHBOUND I-471 FROM I-275 RAMP C TO I-275 RAMP F, REDUCE TRAFFIC TO ONE LANE, SHIFT TO THE INSIDE AND RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 3. CONSTRUCT THE OUTSIDE SHOULDER AND LANE.

SOUTHBOUND I-471 FROM I-275 RAMP F TO I-275 RAMP A, MAINTAIN THREE LANES OF TRAFFIC IN THE AREA BY SHIFTING TRAFFIC TO THE INSIDE, CONSTRUCT THE I-275 RAMP A LANE TAPER FROM I-471 SB, RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 3. INSTALL THE TEMPORARY BARRIER WALL ON RAMP A AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS. CONSTRUCT HALF OF THE OUTSIDE LANE AND ANY PORTION OF THE PERMANENT OUTSIDE SHOULDER NOT CONSTRUCTED IN PHASE 1 AND PERFORM AS MUCH OF THE RAMP A WIDENING WORK AS POSSIBLE WHILE MAINTAINING TRAFFIC ON RAMP A. CLOSE RAMP A AND COMPLETE ALL WORK ON THE RAMP AND THE REMAINING PHASE 3 WORK ON I-471 SB IN THE AREA. SEE THE SECTION TITLED "RAMP CLOSURES" IN THESE NOTES FOR RAMP CLOSURE DETAILS.

I-275 WESTBOUND FROM STA 1197+60 TO STA 1215+40, REDUCE I-275 WB TRAFFIC TO TWO LANES AND SHIFT TRAFFIC ONTO THE INSIDE SHOULDER AND DRIVING LANE, CONSTRUCT THE SLIP RAMP FROM I-275 RAMP A TO I-275 WB AND INSTALL THE TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS. REMOVE THE EXISTING OUTSIDE SHOULDER ON I-275 WB AND CONSTRUCT THE I-275 WB WIDENING AS SHOWN ON THE PROJECT PLANS. WIDEN THE I-275 BRIDGE OVER THREE MILE ROAD AS SHOWN IN THE BRIDGE PLANS.

NORTHBOUND I-471 FROM I-275 RAMP B AND SOUTHBOUND I-471 FROM I-275 RAMP A, TO THE END OF THE PROJECT, REDUCE TRAFFIC TO TWO LANES, SHIFT TRAFFIC TO THE INSIDE LANE AND SHOULDER, CONSTRUCT THE SLIP RAMPS OR TAPERS TO AND FROM THE RAMPS AT THE US 27, GRAND AVENUE, MEMORIAL PARKWAY AND KY 8 INTERCHANGES AND RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 3. CONSTRUCT THE OUTSIDE LANE, HALF OF THE CENTER LANE, THE AUXILIARY LANE (NB AND SB) FROM US 27 TO GRAND AVENUE AND ANY OF THE PERMANENT OUTSIDE SHOULDER NOT CONSTRUCTED IN PHASE 1. CONSTRUCT THE SHOULDER WIDENING AT THE KY 8 RAMP B CORE AREA AND INSTALL THE NEW OUTSIDE SHOULDER BARRIER WALL ON KY 8 RAMP B AND I-471 SB AS SHOWN IN THE PROJECT PLANS. PERFORM THE BRIDGE WORK SHOWN IN THE BRIDGE PROPOSAL IN THE AVAILABLE AREAS ON THE I-471 BRIDGES OVER US 27, GRAND AVENUE, CHESAPEAKE AVENUE, 6TH STREET, KY 8 AND THE OHIO RIVER. CLOSE THE RAMPS AT THE US 27, GRAND AVENUE, MEMORIAL PARKWAY AND KY 8 INTERCHANGES TO COMPLETE ALL WORK ON THE RAMPS AND AREAS ON I-471 THAT WERE NOT CONSTRUCTED WHILE THE RAMPS WERE OPEN. SEE THE SECTIONS TITLED "RAMP CLOSURES" IN THESE NOTES FOR RAMP CLOSURE DETAILS.

PHASE 3A CONSTRUCTION

CONSTRUCT THE I-275 RAMP H ENTRANCE TAPER TO I-471 NB AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS FOR PHASE 3A, SHIFT RAMP H TRAFFIC ONTO THE TAPER AND INSTALL THE TEMPORARY BARRIER WALL. ADJUST THE I-471 NB TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS FOR PHASE 3A AND CONSTRUCT THE REMAINING PHASE 3 WORK IN THE AREA.

CONSTRUCT THE I-275 RAMP A TAPER FROM I-471 SB TO RAMP A AND THE RAMP A SLIP RAMP TO I-275 WB. SHIFT RAMP A TRAFFIC ONTO THE NEWLY CONSTRUCTED LEFT LANE AND PLACE BARRELS ALONG THE RIGHT EDGE OF THE LANE TO MAINTAIN ONE LANE OF TRAFFIC ON THE RAMP. WHEN THE I-275 WB OUTSIDE WIDENING WORK HAS BEEN COMPLETED, RESTORE I-275 WB TRAFFIC TO THE NORMAL THREE LANES, REMOVE THE RAMP A SLIP RAMP TO I-275 WB AND THE BARRELS ALONG THE RIGHT EDGE OR THE DRIVING LANE TO ALLOW FOR THE USE OF BOTH LANES ON THE RAMP.

HIGHLAND AVENUE BRIDGE WORK

THE CONTRACTOR MAY COMPLETE REPLACEMENT OF THE FENCING ON THE HIGHLAND AVENUE BRIDGE, AS SHOWN IN THE BRIDGE PROPOSAL, AT ANY TIME PRIOR TO THE FINAL PROJECT COMPLETION DATE OF DECEMBER 1, 2013. LANE CLOSURES ON HIGHLAND AVE AND I-471 WILL BE REQUIRED DURING REMOVAL AND INSTALLATION OF THE SAFETY FENCE.

RAMP CLOSURES, LANE CLOSURES AND LANE SHIFTS

ALL LANE CLOSURES, LANE SHIFTS AND TAPERS SHALL BE IN ACCORDANCE WITH THE STANDARD DRAWINGS AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD). ANY RAMP CLOSURE, LANE CLOSURE OR LANE SHIFT MUST BE APPROVED BY THE ENGINEER PRIOR TO THE CLOSURE OR LANE SHIFT. THE CONTRACTOR MUST NOTIFY THE ENGINEER AT LEAST FIVE (5) DAYS PRIOR TO ANY PROPOSED LANE CLOSURE OR TRAFFIC PATTERN CHANGE.

SIGNS

THE BID ITEM "SIGNS" WILL BE PAID FOR ON A SQUARE FOOT BASIS AND INCLUDES ALL COSTS NECESSARY TO FURNISH THE SIGNS, RELACEMENT OF THE SIGNS IN THE FIRST LOCATION, RELOCATING THE SIGNS FROM ONE LOCATION TO ANOTHER ACCORDING TO THE CONSTRUCTION PHASE, TEMPORARY COVERING OR REMOVAL OF THE SIGNS WHEN THE SIGN DOES NOT APPLY TO THE WORK BEING PERFORMED, AND UPON COMPLETION OF THE PROJECT, REMOVAL OF THE SIGNS FROM THE JOB SITE.

PERMANENT SIGNING MODIFICATIONS

EXISTING GROUND MOUNTED AND OVERHEAD PANEL SIGNS WILL BE COVERED AS CONSTRUCTION PHASING REQUIRES. THE ENGINEER WILL DETERMINE THE DURATION THAT THE COVERING SHALL REMAIN IN PLACE. SINCE MOST MATERIALS USED IN COVERING SIGNS MAY CAUSE PERMANENT DAMAGE TO THE SIGN FACE, POROUS CLOTH COVERS WHICH ARE FOLDED OVER THE SIGN EDGES AND SECURED ON THE BACK OF THE SIGN MAY BE USED OR THIN GAUGE SHEETING MATERIAL MAY BE USED AS SPECIFIED IN THE FOLLOWING NOTE. THE USE OF TAPE, PAPER, PLASTIC OR SHEET METAL FOR COVERING SIGNS IS STRICTLY PROHIBITED. IF ANY SIGN IS DAMAGED AS A RESULT OF BEING COVERED, THE SIGN SHALL BE REPLACED BY THE CONTRACTOR AT NO COST TO THE DEPARTMENT.

EXISTING SHEETING SIGNS WILL BE REMOVED AND RELOCATED DURING CONSTRUCTION. THE COST FOR REMOVING AND RELOCATING SHEETING SIGNS AND ALL HARDWARE NECESSARY SHALL BE INCIDENTAL TO MAINTAIN AND CONTROL TRAFFIC.

COVERING OF PANEL SIGNS IN KENTUCKY

THIN GAUGE SHEETING MATERIAL MATCHING THE BACKGROUND COLOR ON THE PANEL SIGN MAY BE RIVETED TO THE EXISTING GUIDE SIGNS TO REPRESENT TEMPORARY TRAFFIC FLOW PATTERNS DURING CONSTRUCTION. A SPACER SHALL BE PLACED BETWEEN THE EXISTING SIGN FACE AND BACK OF OVERLAY PANEL TO ALLOW FOR AN AIR CAVITY. SPACERS CAN BE FLAT WASHERS 0.267 ID x 0.625 OD x 0.045 THICK. A 3/16" RIVET SHALL BE USED ON 12" CENTERS AROUND THE PERIMETER OF THE OVERLAY PANEL AND PASS THROUGH THE SPACERS. ALL MATERIAL, EQUIPMENT AND LABOR SHALL BE INCIDENTAL TO MAINTAIN AND CONTROL TRAFFIC.

WHEN COVERING A PANEL SIGN FOR A CLOSED RAMP, A "RAMP CLOSED" SIGN MUST BE PLACED ON THE COVERED SIGN.

ARROW PANELS

THE CONTRACTOR WILL PROVIDE ARROW PANELS AS INDICATED IN THE MAINTENANCE OF TRAFFIC PLAN. THE ARROW PANELS SHALL BE MOUNTED IN TRAFFIC-WORTHY CARRIAGES AND MEET THE REQUIREMENTS AS SPECIFIED IN THE CURRENT STANDARD DRAWINGS. PAYMENT FOR THE ARROW PANELS WILL BE BASED ON A CONTRACT UNIT PRICE OF "EACH" AND WILL BE FULL COMPENSATION FOR PROVIDING, PLACING, OPERATING, RELOCATING AND MAINTAINING THE ARROW PANELS.

THE CONTRACTOR WILL HAVE AVAILABLE ONE RESERVE ARROW PANEL TO BE PLACED INTO OPERATION IN THE EVENT OF DAMAGE OR MECHANICAL/ELECTRICAL FAILURE. NO DIRECT PAYMENT WILL BE ALLOWED FOR THE RESERVE UNIT. ALL ARROW PANELS WILL REMAIN THE PROPERTY OF THE CONTRACTOR UPON COMPLETION OF THE PROJECT.

TRAFFIC CONTROL COORDINATOR

THE PRIME CONTRACTOR IS TO DESIGNATE AN EMPLOYEE WHOSE SOLE RESPONSIBILITY IS TO BE THE TRAFFIC COORDINATOR. THE DESIGNATED TRAFFIC COORDINATOR MUST BE CERTIFIED BY THE AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA). THE TRAFFIC COORDINATOR WILL INSPECT THE PROJECT MAINTENANCE OF TRAFFIC ONCE EVERY TWO HOURS DURING THE CONTRACTOR'S OPERATIONS AND AT ANY TIME A LANE CLOSURE IS IN PLACE. THE TRAFFIC COORDINATOR WILL REPORT ALL INCIDENTS THROUGHOUT THE WORK ZONE TO THE ENGINEER ON THE PROJECT. THE CONTRACTOR WILL FURNISH THE NAME AND TELEPHONE NUMBER WHERE THE TRAFFIC COORDINATOR CAN BE CONTACTED AT ALL TIMES.

DURING ANY PERIOD WHEN A LANE CLOSURE OR TRAFFIC CONTROL DEVICE IS IN PLACE, THE TRAFFIC COORDINATOR WILL ARRANGE FOR PERSONNEL TO BE PRESENT ON THE PROJECT AT ALL TIMES TO INSPECT THE TRAFFIC CONTROL, MAINTAIN THE SIGNING AND DEVICES, AND RELOCATE PORTABLE CHANGEABLE MESSAGE BOARDS AS QUEUE LENGTHS CHANGE. THE PERSONNEL WILL HAVE ACCESS ON THE PROJECT TO A RADIO OR TELEPHONE TO BE USED IN CASE OF EMERGENCIES OR ACCIDENTS.

PAYMENT FOR THE TRAFFIC CONTROL COORDINATOR WILL BE CONSIDERED INCIDENTAL TO THE BID ITEM "MAINTAIN AND CONTROL TRAFFIC".

WRECKER SERVICE

THE CONTRACTOR WILL HAVE CONTINUOUSLY ON CALL A 24-HOUR WRECKER SERVICE THAT THE CONTRACTOR WILL PROMPTLY CONTACT TO REMOVE ANY DISABLED VEHICLE WITHIN THE PROJECT LIMITS. THE WRECKER SERVICE SHOULD HAVE, BUT NOT BE LIMITED TO, THE FOLLOWING ITEMS/CAPABILITIES: CELL PHONE, GASOLINE, JUMPER CABLES, VEHICLE PUSHING, AND TIRE CHANGING. THE CONTRACTOR WILL BE CHARGED THREE THOUSAND DOLLARS (\$3,000) LIQUIDATED DAMAGES FOR EACH 15 MINUTE PERIOD FOR ANY INCIDENCE WHEN THE WRECKER SERVICE FAILS TO REACH THE DISABLED VEHICLE BEYOND THE 30 MINUTE ALLOTTED RESPONSE TIME. PAYMENT FOR THE WRECKER SERVICE WILL BE CONSIDERED INCIDENTAL TO THE BID ITEM "MAINTAIN AND CONTROL TRAFFIC".

LOAD TRANSFER ASSEMBLIES REQUIRED IN JPC SHOULDERS

LOAD TRANSFER ASSEMBLIES WILL BE REQUIRED IN ALL JOINTED PLAIN CONCRETE (JPC) SHOULDERS. PAYMENT FOR PROVIDING AND INSTALLING THESE LOAD TRANSFER ASSEMBLIES WILL BE INCLUDED IN THE UNIT BID PRICE FOR JPC PAVEMENT - 13 IN SHLD.

INSTALLATION AND RELOCATION OF TEMPORARY CONCRETE BARRIER WALL

A LANE CLOSURE MUST BE IN PLACE WHILE INSTALLING OR RELOCATING THE TEMPORARY CONCRETE BARRIER WALL.

TRAFFIC MAY BE REDUCED TO ONE LANE BETWEEN THE HOURS OF 10 PM AND 5 AM TO INSTALL OR ADJUST THE TEMPORARY BARRIER WALL. ANY OTHER TIMES ARE TO BE APPROVED BY THE ENGINEER.

PORTABLE CHANGEABLE MESSAGE SIGNS

PROVIDE PORTABLE CHANGEABLE MESSAGE SIGNS IN ADVANCE OF AND WITHIN THE PROJECT AT LOCATIONS TO BE DETERMINED BY THE ENGINEER. IF MORE THAN ONE LANE CLOSURE IS IN PLACE IN THE SAME DIRECTION OF TRAVEL, PROVIDE ADDITIONAL PORTABLE CHANGEABLE MESSAGE SIGNS. PLACE PORTABLE CHANGEABLE MESSAGE SIGNS ONE MILE IN ADVANCE OF THE ANTICIPATED QUEUE AT EACH LANE CLOSURE SO TRAFFIC HAS WARNING OF SLOWED OR STOPPED TRAFFIC AT LEAST ONE MILE BUT NOT MORE THAN TWO MILES BEFORE REACHING THE END OF THE ACTUAL QUEUE. THE LOCATIONS DESIGNATED MAY VARY AS THE WORK PROGRESSES. THE ENGINEER WILL DESIGNATE WHICH MESSAGES ARE REQUIRED. IN THE EVENT OF DAMAGE OR MECHANICAL OR ELECTRICAL FAILURE, THE CONTRACTOR SHALL REPAIR OR REPLACE THE PORTABLE CHANGEABLE MESSAGE SIGN IMMEDIATELY.

TRUCK MOUNTED ATTENUATORS

FURNISH AND INSTALL TRUCK MOUNTED ATTENUATORS (TMA'S) IN ADVANCE OF ALL PAVEMENT REMOVAL AREAS. IF THERE IS LESS THAN 500 FEET BETWEEN WORK SITES, ONLY A SINGLE TMA WILL BE REQUIRED AT A LOCATION AS DIRECTED BY THE ENGINEER. THE TMA'S SHALL BE LOCATED AT THE INDIVIDUAL WORK SITES AND SHALL BE MOVED AS THE WORK ZONE MOVES WITHIN THE PROJECT LIMITS. ALL DETAILS OF THE TMA INSTALLATIONS ARE TO BE APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL RETAIN OWNERSHIP OF THE TMA'S AT THE COMPLETION OF THE PROJECT. TMA'S ARE INCIDENTAL TO "MAINTAIN AND CONTROL TRAFFIC".

CONTRACTOR'S AND CONTRACTOR'S EMPLOYEE'S VEHICLES

CHANGE DIRECTION OF TRAVEL ONLY AT INTERCHANGES. MEDIAN CROSSOVERS MAY NOT BE USED UNLESS A REQUEST FOR THEIR USE IS SUBMITTED TO AND APPROVED BY THE ENGINEER. THE REQUEST MUST DELINEATE THE TYPE OF VEHICLE(S) THE REQUEST IS MADE FOR, DURATION OF PROPOSED USE OF THE CROSSOVER, SPECIFIC MAINTENANCE OF TRAFFIC CONTROL TO BE USED TO INSURE THE SAFETY OF THE TRAVELING PUBLIC AS WELL AS THE CONTRACTOR'S EMPLOYEES AND ANY OTHER ITEM THE ENGINEER REQUESTS.

CONCRETE BARRIER WALL DELINEATORS

THE CONTRACTOR SHALL PROVIDE YELLOW AND WHITE DELINEATORS FOR CONCRETE BARRIERS ON BRIDGES AS SHOWN IN CURRENT STANDARD DRAWING RBM-020. THE DELINEATORS ARE TO BE INSTALLED ON THE CONCRETE BARRIERS OF THE I-275 BRIDGES AND THE TEMPORARY CONCRETE BARRIER WALL TYPE 9T. PAYMENT WILL BE BASED ON A CONTRACT UNIT PRICE FOR "EACH" AND WILL BE FULL COMPENSATION FOR PROVIDING AND INSTALLING THE CONCRETE BARRIER WALL DELINEATORS. DELINEATORS PLACED ON TEMPORARY CONCRETE BARRIER WALL TYPE 9T WILL ONLY BE PAID FOR IF THEY ARE NEW DELINEATORS INSTALLED FOR THE CURRENT PROJECT. ANY EXISTING DELINEATORS ON THE TEMPORARY WALL WHICH ARE IN GOOD CONDITION WILL NOT BE PAID FOR BUT WILL BE INCIDENTAL TO MAINTAIN AND CONTROL TRAFFIC. SPACING OF DELINEATORS ON THE BRIDGE BARRIERS IS TO BE AS SHOWN IN CURRENT STANDARD DRAWING RBM-020. ONE DELINEATOR IS REQUIRED ON EACH 20 FOOT SECTION OF TEMPORARY CONCRETE BARRIER WALL TYPE 9T. DELINEATORS PLACED ON THE BRIDGE BARRIERS ARE TO REMAIN IN PLACE AT COMPLETION OF THE PROJECT. DELINEATORS PLACED ON TEMPORARY CONCRETE BARRIER WALL TYPE 9T WILL BECOME THE PROPERTY OF THE CONTRACTOR AT COMPLETION OF THE PROJECT.

GUARDRAIL DELINEATORS

THE CONTRACTOR WILL PROVIDE YELLOW AND WHITE DELINEATORS FOR GUARDRAIL AS SHOWN IN THE DETAILS ON SHEET R138, "DELINEATORS FOR GUARDRAIL". THESE DELINEATORS WILL BE PLACED ON THE EXISTING GUARDRAIL FOR MAINTENANCE OF TRAFFIC PURPOSES AND THEN TRANSFERRED TO THE PERMANENT GUARDRAIL FOR ADDITIONAL DELINEATION OF THE FINAL ROADWAY. SPACING OF THE DELINEATORS WILL BE PER THE SPACING TABLE ON SHEET R101 EXCEPT THE MAXIMUM SPACING WILL BE 50' ON CENTERS WHEN TRAFFIC WILL BE LESS THAN EIGHT (8) FEET FROM THE GUARDRAIL. PAYMENT WILL BE BASED ON A CONTRACT UNIT PRICE OF "EACH" AND WILL BE FULL COMPENSATION FOR PROVIDING, INSTALLING THE DELINEATORS FOR MAINTENANCE OF TRAFFIC ON THE EXISTING GUARDRAIL AND THEN MOVING THEM TO THE PERMANENT GUARDRAIL AS INSTRUCTED BY THE ENGINEER.

MAINTENANCE OF TRAFFIC NOTES

COUNTY OF	ITEM NO.	SHEET NO.
CAMPBELL	6-2021.00	R146

FILE NAME: P:\CIVIL\1471\6-2021.00\CONTRACT PLANS AND PROPOSAL REVISIONS AFTER SUBMITTAL\PLANS REVISION 3\146000T.DGN
 USER: fropier
 DATE PLOTTED: February 10, 2012
 E-SHEET NAME:
 MicroStation v8.11.7.443

PHASE 2A CONSTRUCTION

CONSTRUCT THE TEMPORARY I-275 RAMP F EXIT TAPER AND LANE FROM I-471 SOUTHBOUND ON THE NEWLY CONSTRUCTED INSIDE SHOULDER, INSTALL THE TEMPORARY BARRIER WALL ON THE TEMPORARY RAMP F EXIT LANE AND SHIFT RAMP F TRAFFIC ONTO THE EXIT LANE. REPOSITION THE TEMPORARY BARRIER WALL ON I-471 SOUTHBOUND INTO THE NEW LOCATION AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS FOR PHASE 2A. CONSTRUCT THE REMAINING PHASE 2 WORK IN THE AREA. △

PHASE 3 CONSTRUCTION

NORTHBOUND I-471 FROM THE BEGINNING OF THE PROJECT AT THE I-471/US 27 INTERSECTION TO THE END OF THE BARRIER WALL (JUST NORTH OF THE BEGINNING OF THE I-471 NB/SB BIFURCATION), REDUCE TRAFFIC TO ONE LANE AND SHIFT ONTO THE INSIDE LANE AND SHOULDER, RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 3. CONSTRUCT THE OUTSIDE SHOULDER AND HALF OF THE OUTSIDE LANE.

NORTHBOUND I-471 FROM THE END OF THE BARRIER WALL JUST NORTH OF THE I-471 NB/SB BIFURCATION TO RAMP B AT THE I-471/I-275 INTERCHANGE, REDUCE TRAFFIC TO ONE LANE, SHIFT TRAFFIC TO THE INSIDE, CONSTRUCT THE SLIP RAMP TO I-275 RAMP H, THE TAPER TO I-275 RAMP G AND RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 3. CONSTRUCT THE OUTSIDE LANE AND SHOULDER IN THE AVAILABLE AREAS WHILE MAINTAINING TRAFFIC TO RAMP G AND H. CLOSE RAMP G AND CONSTRUCT THE REMAINING PHASE 3 WORK IN THE RAMP AREA. SEE THE SECTION TITLED "RAMP CLOSURES" IN THESE NOTES FOR RAMP CLOSURE DETAILS.

SOUTHBOUND I-471 FROM THE BEGINNING OF THE PROJECT AT THE I-471/US 27 INTERSECTION TO JUST NORTH OF THE BEGINNING OF THE I-471 NB/SB BIFURCATION, REDUCE TRAFFIC TO TWO LANES AND SHIFT TO INSIDE LANE AND SHOULDER AND RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 3. CONSTRUCT THE OUTSIDE LANE AND ANY PERMANENT OUTSIDE SHOULDER NOT CONSTRUCTED IN PHASE 1. REMOVE THE TEMPORARY SHOULDER WIDENING AND INSTALL GUARDRAIL AT THE SAME LOCATION IT WAS PRIOR TO REMOVAL IN PHASE 1.

SOUTHBOUND I-471 FROM JUST NORTH OF THE I-471 NB/SB BIFURCATION TO I-275 RAMP C, REDUCE TRAFFIC TO TWO LANES (WITH TRAFFIC FROM RAMP C BECOMING THE SECOND LANE), SHIFT TRAFFIC TO THE INSIDE, CONSTRUCT THE SLIP RAMP FROM RAMP C AND THE I-275 RAMP D TAPER TO I-471 SB AND RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS. CONSTRUCT THE OUTSIDE LANE, HALF OF THE CENTER LANE AND ANY PART OF THE PERMANENT OUTSIDE SHOULDER NOT CONSTRUCTED IN PHASE 1 IN THE AVAILABLE AREAS WHILE MAINTAINING TRAFFIC ON RAMP C. CLOSE RAMP C AND CONSTRUCT THE REMAINING PHASE 3 WORK IN THE AREA. SEE THE SECTION TITLED "RAMP CLOSURES" IN THESE NOTES FOR RAMP CLOSURE DETAILS.

SOUTHBOUND I-471 FROM I-275 RAMP C TO I-275 RAMP F, REDUCE TRAFFIC TO ONE LANE, SHIFT TO THE INSIDE AND RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 3. CONSTRUCT THE OUTSIDE SHOULDER AND LANE.

SOUTHBOUND I-471 FROM I-275 RAMP F TO I-275 RAMP A, MAINTAIN THREE LANES OF TRAFFIC IN THE AREA BY SHIFTING TRAFFIC TO THE INSIDE, CONSTRUCT THE I-275 RAMP A LANE TAPER FROM I-471 SB, RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 3. INSTALL THE TEMPORARY BARRIER WALL ON RAMP A AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS. CONSTRUCT HALF OF THE OUTSIDE LANE AND ANY PORTION OF THE PERMANENT OUTSIDE SHOULDER NOT CONSTRUCTED IN PHASE 1 AND PERFORM AS MUCH OF THE RAMP A WIDENING WORK AS POSSIBLE WHILE MAINTAINING TRAFFIC ON RAMP A. CLOSE RAMP A AND COMPLETE ALL WORK ON THE RAMP AND THE REMAINING PHASE 3 WORK ON I-471 SB IN THE AREA. SEE THE SECTION TITLED "RAMP CLOSURES" IN THESE NOTES FOR RAMP CLOSURE DETAILS.

I-275 WESTBOUND FROM STA 1197+60 TO STA 1215+40, REDUCE I-275 WB TRAFFIC TO TWO LANES AND SHIFT TRAFFIC ONTO THE INSIDE SHOULDER AND DRIVING LANE, CONSTRUCT THE SLIP RAMP FROM I-275 RAMP A TO I-275 WB AND INSTALL THE TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS. REMOVE THE EXISTING OUTSIDE SHOULDER ON I-275 WB AND CONSTRUCT THE I-275 WB WIDENING AS SHOWN ON THE PROJECT PLANS. WIDEN THE I-275 BRIDGE OVER THREE MILE ROAD AS SHOWN IN THE BRIDGE PLANS.

NORTHBOUND I-471 FROM I-275 RAMP B AND SOUTHBOUND I-471 FROM I-275 RAMP A, TO THE END OF THE PROJECT, REDUCE TRAFFIC TO TWO LANES, SHIFT TRAFFIC TO THE INSIDE LANE AND SHOULDER, CONSTRUCT THE SLIP RAMPS OR TAPERS TO AND FROM THE RAMPS AT THE US 27, GRAND AVENUE, MEMORIAL PARKWAY AND KY 8 INTERCHANGES AND RELOCATE THE PHASE 2 TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS FOR PHASE 3. CONSTRUCT THE OUTSIDE LANE, HALF OF THE CENTER LANE, THE AUXILIARY LANE (NB AND SB) FROM US 27 TO GRAND AVENUE AND ANY OF THE PERMANENT OUTSIDE SHOULDER NOT CONSTRUCTED IN PHASE 1. CONSTRUCT THE SHOULDER WIDENING AT THE KY 8 RAMP B CORE AREA AND INSTALL THE NEW OUTSIDE SHOULDER BARRIER WALL ON KY 8 RAMP B AND I-471 SB AS SHOWN IN THE PROJECT PLANS. PERFORM THE BRIDGE WORK SHOWN IN THE BRIDGE PROPOSAL IN THE AVAILABLE AREAS ON THE I-471 BRIDGES OVER US 27, GRAND AVENUE, CHESAPEAKE AVENUE, 6TH STREET, KY 8 AND THE OHIO RIVER. CLOSE THE RAMPS AT THE US 27, GRAND AVENUE, MEMORIAL PARKWAY AND KY 8 INTERCHANGES TO COMPLETE ALL WORK ON THE RAMPS AND AREAS ON I-471 THAT WERE NOT CONSTRUCTED WHILE THE RAMPS WERE OPEN. SEE THE SECTIONS TITLED "RAMP CLOSURES" IN THESE NOTES FOR RAMP CLOSURE DETAILS.

PHASE 3A CONSTRUCTION

CONSTRUCT THE I-275 RAMP H ENTRANCE TAPER TO I-471 NB AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS FOR PHASE 3A, SHIFT RAMP H TRAFFIC ONTO THE TAPER AND INSTALL THE TEMPORARY BARRIER WALL. ADJUST THE I-471 NB TEMPORARY BARRIER WALL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS FOR PHASE 3A AND CONSTRUCT THE REMAINING PHASE 3 WORK IN THE AREA.

CONSTRUCT THE I-275 RAMP A TAPER FROM I-471 SB TO RAMP A AND THE RAMP A SLIP RAMP TO I-275 WB. SHIFT RAMP A TRAFFIC ONTO THE NEWLY CONSTRUCTED LEFT LANE AND PLACE BARRELS ALONG THE RIGHT EDGE OF THE LANE TO MAINTAIN ONE LANE OF TRAFFIC ON THE RAMP. WHEN THE I-275 WB OUTSIDE WIDENING WORK HAS BEEN COMPLETED, RESTORE I-275 WB TRAFFIC TO THE NORMAL THREE LANES, REMOVE THE RAMP A SLIP RAMP TO I-275 WB AND THE BARRELS ALONG THE RIGHT EDGE OR THE DRIVING LANE TO ALLOW FOR THE USE OF BOTH LANES ON THE RAMP.

HIGHLAND AVENUE BRIDGE WORK

THE CONTRACTOR MAY COMPLETE REPLACEMENT OF THE FENCING ON THE HIGHLAND AVENUE BRIDGE, AS SHOWN IN THE BRIDGE PROPOSAL, AT ANY TIME PRIOR TO THE FINAL PROJECT COMPLETION DATE OF DECEMBER 1, 2013. LANE CLOSURES ON HIGHLAND AVE AND I-471 WILL BE REQUIRED DURING REMOVAL AND INSTALLATION OF THE SAFETY FENCE.

RAMP CLOSURES, LANE CLOSURES AND LANE SHIFTS

ALL LANE CLOSURES, LANE SHIFTS AND TAPERS SHALL BE IN ACCORDANCE WITH THE STANDARD DRAWINGS AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD). ANY RAMP CLOSURE, LANE CLOSURE OR LANE SHIFT MUST BE APPROVED BY THE ENGINEER PRIOR TO THE CLOSURE OR LANE SHIFT. THE CONTRACTOR MUST NOTIFY THE ENGINEER AT LEAST FIVE (5) DAYS PRIOR TO ANY PROPOSED LANE CLOSURE OR TRAFFIC PATTERN CHANGE.

SIGNS

THE BID ITEM "SIGNS" WILL BE PAID FOR ON A SQUARE FOOT BASIS AND INCLUDES ALL COSTS NECESSARY TO FURNISH THE SIGNS, RELACEMENT OF THE SIGNS IN THE FIRST LOCATION, RELOCATING THE SIGNS FROM ONE LOCATION TO ANOTHER ACCORDING TO THE CONSTRUCTION PHASE, TEMPORARY COVERING OR REMOVAL OF THE SIGNS WHEN THE SIGN DOES NOT APPLY TO THE WORK BEING PERFORMED, AND UPON COMPLETION OF THE PROJECT, REMOVAL OF THE SIGNS FROM THE JOB SITE.

PERMANENT SIGNING MODIFICATIONS

EXISTING GROUND MOUNTED AND OVERHEAD PANEL SIGNS WILL BE COVERED AS CONSTRUCTION PHASING REQUIRES. THE ENGINEER WILL DETERMINE THE DURATION THAT THE COVERING SHALL REMAIN IN PLACE. SINCE MOST MATERIALS USED IN COVERING SIGNS MAY CAUSE PERMANENT DAMAGE TO THE SIGN FACE, POROUS CLOTH COVERS WHICH ARE FOLDED OVER THE SIGN EDGES AND SECURED ON THE BACK OF THE SIGN MAY BE USED OR THIN GAUGE SHEETING MATERIAL MAY BE USED AS SPECIFIED IN THE FOLLOWING NOTE. THE USE OF TAPE, PAPER, PLASTIC OR SHEET METAL FOR COVERING SIGNS IS STRICTLY PROHIBITED. IF ANY SIGN IS DAMAGED AS A RESULT OF BEING COVERED, THE SIGN SHALL BE REPLACED BY THE CONTRACTOR AT NO COST TO THE DEPARTMENT.

EXISTING SHEETING SIGNS WILL BE REMOVED AND RELOCATED DURING CONSTRUCTION. THE COST FOR REMOVING AND RELOCATING SHEETING SIGNS AND ALL HARDWARE NECESSARY SHALL BE INCIDENTAL TO MAINTAIN AND CONTROL TRAFFIC.

COVERING OF PANEL SIGNS IN KENTUCKY

THIN GAUGE SHEETING MATERIAL MATCHING THE BACKGROUND COLOR ON THE PANEL SIGN MAY BE RIVETED TO THE EXISTING GUIDE SIGNS TO REPRESENT TEMPORARY TRAFFIC FLOW PATTERNS DURING CONSTRUCTION. A SPACER SHALL BE PLACED BETWEEN THE EXISTING SIGN FACE AND BACK OF OVERLAY PANEL TO ALLOW FOR AN AIR CAVITY. SPACERS CAN BE FLAT WASHERS 0.267 ID x 0.625 OD x 0.045 THICK. A 3/16" RIVET SHALL BE USED ON 12" CENTERS AROUND THE PERIMETER OF THE OVERLAY PANEL AND PASS THROUGH THE SPACERS. ALL MATERIAL, EQUIPMENT AND LABOR SHALL BE INCIDENTAL TO MAINTAIN AND CONTROL TRAFFIC.

WHEN COVERING A PANEL SIGN FOR A CLOSED RAMP, A "RAMP CLOSED" SIGN MUST BE PLACED ON THE COVERED SIGN.

ARROW PANELS

THE CONTRACTOR WILL PROVIDE ARROW PANELS AS INDICATED IN THE MAINTENANCE OF TRAFFIC PLAN. THE ARROW PANELS SHALL BE MOUNTED IN TRAFFIC-WORTHY CARRIAGES AND MEET THE REQUIREMENTS AS SPECIFIED IN THE CURRENT STANDARD DRAWINGS. PAYMENT FOR THE ARROW PANELS WILL BE BASED ON A CONTRACT UNIT PRICE OF "EACH" AND WILL BE FULL COMPENSATION FOR PROVIDING, PLACING, OPERATING, RELOCATING AND MAINTAINING THE ARROW PANELS.

THE CONTRACTOR WILL HAVE AVAILABLE ONE RESERVE ARROW PANEL TO BE PLACED INTO OPERATION IN THE EVENT OF DAMAGE OR MECHANICAL/ELECTRICAL FAILURE. NO DIRECT PAYMENT WILL BE ALLOWED FOR THE RESERVE UNIT. ALL ARROW PANELS WILL REMAIN THE PROPERTY OF THE CONTRACTOR UPON COMPLETION OF THE PROJECT.

TRAFFIC CONTROL COORDINATOR

THE PRIME CONTRACTOR IS TO DESIGNATE AN EMPLOYEE WHOSE SOLE RESPONSIBILITY IS TO BE THE TRAFFIC COORDINATOR. THE DESIGNATED TRAFFIC COORDINATOR MUST BE CERTIFIED BY THE AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA). THE TRAFFIC COORDINATOR WILL INSPECT THE PROJECT MAINTENANCE OF TRAFFIC ONCE EVERY TWO HOURS DURING THE CONTRACTOR'S OPERATIONS AND AT ANY TIME A LANE CLOSURE IS IN PLACE. THE TRAFFIC COORDINATOR WILL REPORT ALL INCIDENTS THROUGHOUT THE WORK ZONE TO THE ENGINEER ON THE PROJECT. THE CONTRACTOR WILL FURNISH THE NAME AND TELEPHONE NUMBER WHERE THE TRAFFIC COORDINATOR CAN BE CONTACTED AT ALL TIMES.

DURING ANY PERIOD WHEN A LANE CLOSURE OR TRAFFIC CONTROL DEVICE IS IN PLACE, THE TRAFFIC COORDINATOR WILL ARRANGE FOR PERSONNEL TO BE PRESENT ON THE PROJECT AT ALL TIMES TO INSPECT THE TRAFFIC CONTROL, MAINTAIN THE SIGNING AND DEVICES, AND RELOCATE PORTABLE CHANGEABLE MESSAGE BOARDS AS QUEUE LENGTHS CHANGE. THE PERSONNEL WILL HAVE ACCESS ON THE PROJECT TO A RADIO OR TELEPHONE TO BE USED IN CASE OF EMERGENCIES OR ACCIDENTS.

PAYMENT FOR THE TRAFFIC CONTROL COORDINATOR WILL BE CONSIDERED INCIDENTAL TO THE BID ITEM "MAINTAIN AND CONTROL TRAFFIC".

WRECKER SERVICE

THE CONTRACTOR WILL HAVE CONTINUOUSLY ON CALL A 24-HOUR WRECKER SERVICE THAT THE CONTRACTOR WILL PROMPTLY CONTACT TO REMOVE ANY DISABLED VEHICLE WITHIN THE PROJECT LIMITS. THE WRECKER SERVICE SHOULD HAVE, BUT NOT BE LIMITED TO, THE FOLLOWING ITEMS/CAPABILITIES: CELL PHONE, GASOLINE, JUMPER CABLES, VEHICLE PUSHING, AND TIRE CHANGING. THE CONTRACTOR WILL BE CHARGED THREE THOUSAND DOLLARS (\$3,000) LIQUIDATED DAMAGES FOR EACH 15 MINUTE PERIOD FOR ANY INCIDENCE WHEN THE WRECKER SERVICE FAILS TO REACH THE DISABLED VEHICLE BEYOND THE 30 MINUTE ALLOTTED RESPONSE TIME. PAYMENT FOR THE WRECKER SERVICE WILL BE CONSIDERED INCIDENTAL TO THE BID ITEM "MAINTAIN AND CONTROL TRAFFIC".

LOAD TRANSFER ASSEMBLIES REQUIRED IN JPC SHOULDERS

LOAD TRANSFER ASSEMBLIES WILL BE REQUIRED IN ALL JOINTED PLAIN CONCRETE (JPC) SHOULDERS. PAYMENT FOR PROVIDING AND INSTALLING THESE LOAD TRANSFER ASSEMBLIES WILL BE INCLUDED IN THE UNIT BID PRICE FOR JPC PAVEMENT - 13 IN SHLD.

INSTALLATION AND RELOCATION OF TEMPORARY CONCRETE BARRIER WALL

A LANE CLOSURE MUST BE IN PLACE WHILE INSTALLING OR RELOCATING THE TEMPORARY CONCRETE BARRIER WALL.

TRAFFIC MAY BE REDUCED TO ONE LANE BETWEEN THE HOURS OF 10 PM AND 5 AM TO INSTALL OR ADJUST THE TEMPORARY BARRIER WALL. ANY OTHER TIMES ARE TO BE APPROVED BY THE ENGINEER.

PORTABLE CHANGEABLE MESSAGE SIGNS

PROVIDE PORTABLE CHANGEABLE MESSAGE SIGNS IN ADVANCE OF AND WITHIN THE PROJECT AT LOCATIONS TO BE DETERMINED BY THE ENGINEER. IF MORE THAN ONE LANE CLOSURE IS IN PLACE IN THE SAME DIRECTION OF TRAVEL, PROVIDE ADDITIONAL PORTABLE CHANGEABLE MESSAGE SIGNS. PLACE PORTABLE CHANGEABLE MESSAGE SIGNS ONE MILE IN ADVANCE OF THE ANTICIPATED QUEUE AT EACH LANE CLOSURE SO TRAFFIC HAS WARNING OF SLOWED OR STOPPED TRAFFIC AT LEAST ONE MILE BUT NOT MORE THAN TWO MILES BEFORE REACHING THE END OF THE ACTUAL QUEUE. THE LOCATIONS DESIGNATED MAY VARY AS THE WORK PROGRESSES. THE ENGINEER WILL DESIGNATE WHICH MESSAGES ARE REQUIRED. IN THE EVENT OF DAMAGE OR MECHANICAL OR ELECTRICAL FAILURE, THE CONTRACTOR SHALL REPAIR OR REPLACE THE PORTABLE CHANGEABLE MESSAGE SIGN IMMEDIATELY.

TRUCK MOUNTED ATTENUATORS

FURNISH AND INSTALL TRUCK MOUNTED ATTENUATORS (TMA'S) IN ADVANCE OF ALL PAVEMENT REMOVAL AREAS. IF THERE IS LESS THAN 500 FEET BETWEEN WORK SITES, ONLY A SINGLE TMA WILL BE REQUIRED AT A LOCATION AS DIRECTED BY THE ENGINEER. THE TMA'S SHALL BE LOCATED AT THE INDIVIDUAL WORK SITES AND SHALL BE MOVED AS THE WORK ZONE MOVES WITHIN THE PROJECT LIMITS. ALL DETAILS OF THE TMA INSTALLATIONS ARE TO BE APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL RETAIN OWNERSHIP OF THE TMA'S AT THE COMPLETION OF THE PROJECT. TMA'S ARE INCIDENTAL TO "MAINTAIN AND CONTROL TRAFFIC".

CONTRACTOR'S AND CONTRACTOR'S EMPLOYEE'S VEHICLES

CHANGE DIRECTION OF TRAVEL ONLY AT INTERCHANGES. MEDIAN CROSSOVERS MAY NOT BE USED UNLESS A REQUEST FOR THEIR USE IS SUBMITTED TO AND APPROVED BY THE ENGINEER. THE REQUEST MUST DELINEATE THE TYPE OF VEHICLE(S) THE REQUEST IS MADE FOR, DURATION OF PROPOSED USE OF THE CROSSOVER, SPECIFIC MAINTENANCE OF TRAFFIC CONTROL TO BE USED TO INSURE THE SAFETY OF THE TRAVELING PUBLIC AS WELL AS THE CONTRACTOR'S EMPLOYEES AND ANY OTHER ITEM THE ENGINEER REQUESTS.

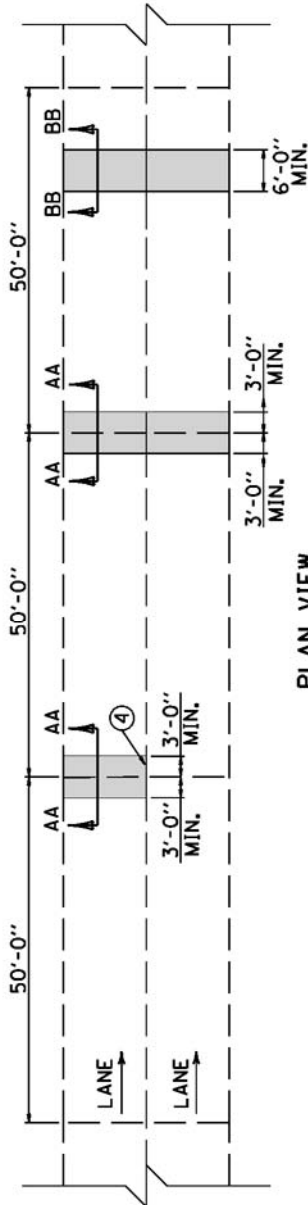
CONCRETE BARRIER WALL DELINEATORS

THE CONTRACTOR SHALL PROVIDE YELLOW AND WHITE DELINEATORS FOR CONCRETE BARRIERS ON BRIDGES AS SHOWN IN CURRENT STANDARD DRAWING RBM-020. THE DELINEATORS ARE TO BE INSTALLED ON THE CONCRETE BARRIERS OF THE I-275 BRIDGES AND THE TEMPORARY CONCRETE BARRIER WALL TYPE 9T. PAYMENT WILL BE BASED ON A CONTRACT UNIT PRICE FOR "EACH" AND WILL BE FULL COMPENSATION FOR PROVIDING AND INSTALLING THE CONCRETE BARRIER WALL DELINEATORS. DELINEATORS PLACED ON TEMPORARY CONCRETE BARRIER WALL TYPE 9T WILL ONLY BE PAID FOR IF THEY ARE NEW DELINEATORS INSTALLED FOR THE CURRENT PROJECT. ANY EXISTING DELINEATORS ON THE TEMPORARY WALL WHICH ARE IN GOOD CONDITION WILL NOT BE PAID FOR BUT WILL BE INCIDENTAL TO MAINTAIN AND CONTROL TRAFFIC. SPACING OF DELINEATORS ON THE BRIDGE BARRIERS IS TO BE AS SHOWN IN CURRENT STANDARD DRAWING RBM-020. ONE DELINEATOR IS REQUIRED ON EACH 20 FOOT SECTION OF TEMPORARY CONCRETE BARRIER WALL TYPE 9T. DELINEATORS PLACED ON THE BRIDGE BARRIERS ARE TO REMAIN IN PLACE AT COMPLETION OF THE PROJECT. DELINEATORS PLACED ON TEMPORARY CONCRETE BARRIER WALL TYPE 9T WILL BE BECOME THE PROPERTY OF THE CONTRACTOR AT COMPLETION OF THE PROJECT.

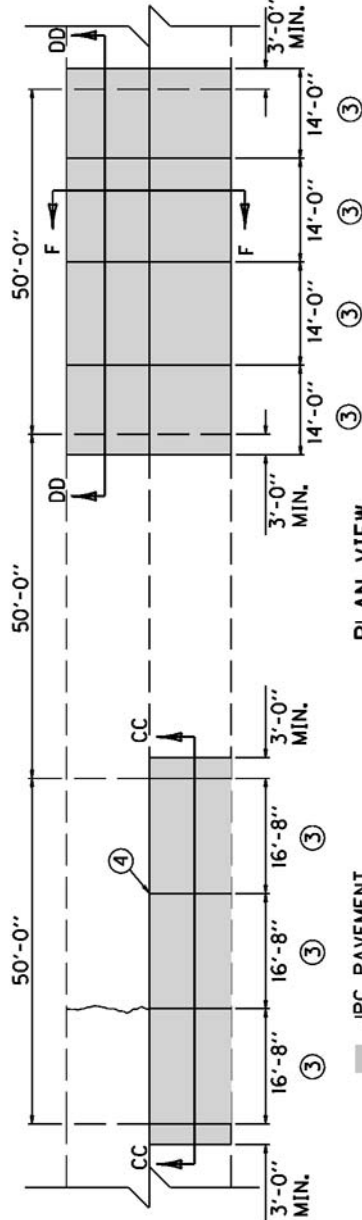
GUARDRAIL DELINEATORS

THE CONTRACTOR WILL PROVIDE YELLOW AND WHITE DELINEATORS FOR GUARDRAIL AS SHOWN IN THE DETAILS ON SHEET R138, "DELINEATORS FOR GUARDRAIL". THESE DELINEATORS WILL BE PLACED ON THE EXISTING GUARDRAIL FOR MAINTENANCE OF TRAFFIC PURPOSES AND THEN TRANSFERRED TO THE PERMANENT GUARDRAIL FOR ADDITIONAL DELINEATION OF THE FINAL ROADWAY. SPACING OF THE DELINEATORS WILL BE PER THE SPACING TABLE ON SHEET R101 EXCEPT THE MAXIMUM SPACING WILL BE 50' ON CENTERS WHEN TRAFFIC WILL BE LESS THAN EIGHT (8) FEET FROM THE GUARDRAIL. PAYMENT WILL BE BASED ON A CONTRACT UNIT PRICE OF "EACH" AND WILL BE FULL COMPENSATION FOR PROVIDING, INSTALLING THE DELINEATORS FOR MAINTENANCE OF TRAFFIC ON THE EXISTING GUARDRAIL AND THEN MOVING THEM TO THE PERMANENT GUARDRAIL AS INSTRUCTED BY THE ENGINEER.

1. SAW AT LOCATIONS "J" AND ALONG LONGITUDINAL JOINT (IF ONLY ONE LANE IS REMOVED) FULL DEPTH JOINTS AS TO THE ENGINEER DIRECTS OR APPROVES. REMOVE THE EXISTING JPC PAVEMENT TO THE LENGTH AND AT THE LOCATIONS NOTED ELSEWHERE IN THE CONTRACT. L=6 FEET MINIMUM AND LOCATIONS "J" SHALL NOT BE CLOSER THAN 6 FEET TO ANY TRANSVERSE JOINT BEYOND THE REPAIR.
2. INSTALL SMOOTH, LOAD TRANSFER DOWELS (EXCEPT USE TIE BARS FOR SECTION CC), 18 INCHES LONG (SEE STANDARD DRAWING NO. RPS-020 FOR DOWEL SIZE) AT LOCATIONS "J". INSTALL DOWELS OR TIE BARS FOR SECTION CC IN THE EXISTING CONCRETE USING EPOXY TYPE IV. INSTALL DOWELS OR TIE BARS FOR SECTION CC ON 12 INCH CENTERS BEGINNING 12 INCHES FROM THE EDGE OF THE SLAB.
3. IF L IS GREATER THAN 20 FEET, INSTALL NEW LOAD TRANSFER ASSEMBLY(S) AND CONSTRUCT CONTRACTION JOINTS SUCH THAT THE DISTANCE BETWEEN JOINTS IN THE REPLACED SECTION IS NO LESS THAN 10 FEET OR MORE THAN 20 FEET. TRANSVERSE JOINTS SHALL BE SPACED APPROXIMATELY 15' EQUIDISTANT, BUT NOT LESS THAN 10 FEET OR NO MORE THAN 20 FEET. ADJUST JOINTS TO PROVIDE THE MINIMUM NUMBER OF JOINTS WITHOUT EXCEEDING THE 10-20 FOOT RANGE. INSTALL NEW LOAD TRANSFER ASSEMBLY(S) AND ALIGN LOAD TRANSFER ASSEMBLY(S) WITH AN EXISTING JOINT OR CRACK IN THE ADJACENT SLAB IF ONLY ONE LANE IS BEING REPLACED.
4. IF ONLY ONE LANE IS REMOVED, AND L > 25', INSTALL NEW 1-INCH TIE BARS 18 INCHES LONG ON 30 INCH CENTERS IN THE LONGITUDINAL JOINT USING EPOXY TYPE IV. IF 2 OR MORE LANES ARE REMOVED, CONSTRUCT LONGITUDINAL JOINT(S) ACCORDING TO THE STANDARD DRAWING EXCEPT USE 1-INCH TIE BARS 18 INCHES LONG ON 30 INCH CENTERS. IF L > 25', DO NOT TIE THE LONGITUDINAL JOINT TO THE EXISTING LANE; USE A BOND BREAKER MATERIAL APPROVED BY THE ENGINEER THAT WILL ASSURE NO INTERACTION WITH THE ADJACENT LANE.
5. REPLACE WITH NON-REINFORCED JPC PAVEMENT AND INSTALL CONTRACTION JOINTS AT LOCATIONS "K" AND CONTRACTION JOINTS FOR A CONSTRUCTION JOINT FOR LOCATION CC AT LOCATIONS "J". SAW AND SEAL ALL JOINTS.
6. SEE "CROSS SECTION" FOR SECTION F.

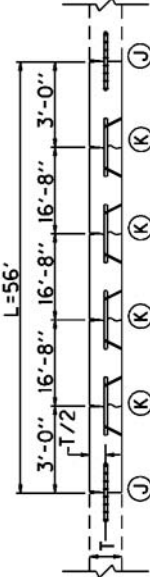


PLAN VIEW



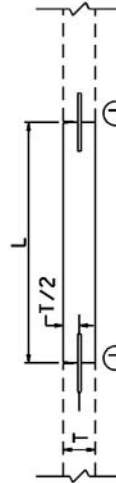
PLAN VIEW

■ JPC PAVEMENT TO BE REMOVED



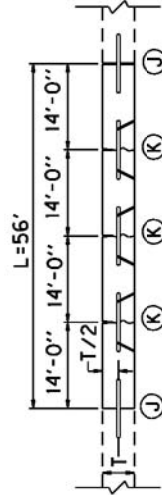
SECTION CC

LANE REPLACEMENT WHERE ADJACENT LANES OR JPC SHOULDERS WILL REMAIN



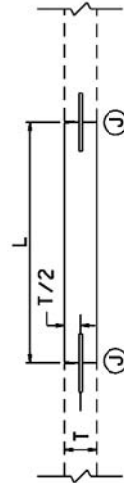
SECTION AA

JOINT REPLACEMENT



SECTION DD

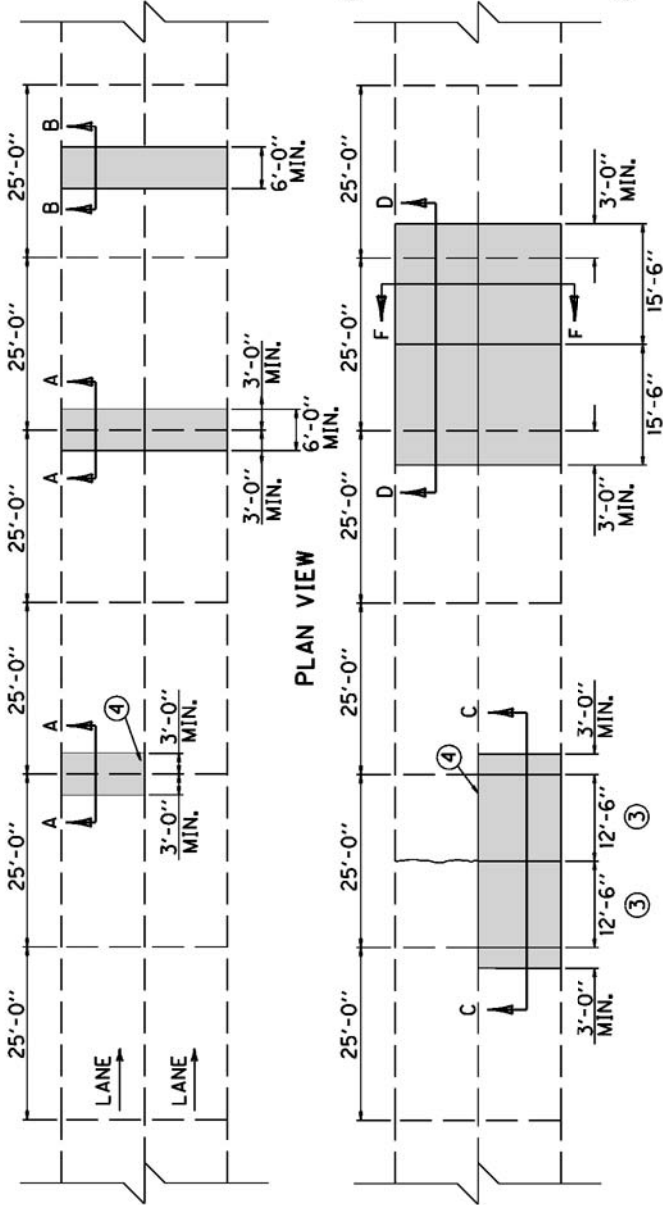
FULL WIDTH REPLACEMENT (INCLUDING JPC SHOULDERS)



SECTION BB

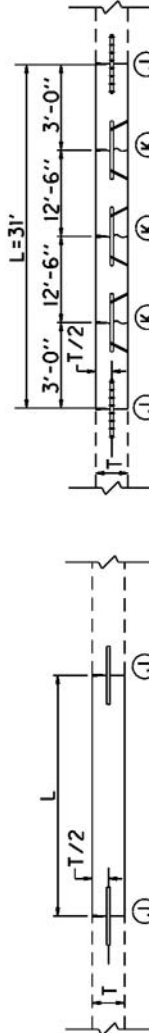
MID-SLAB REPLACEMENT

1. SAW AT LOCATIONS "J" AND ALONG LONGITUDINAL JOINT (IF ONLY ONE LANE IS REMOVED) FULL DEPTH WITHOUT DAMAGE TO EXISTING CONCRETE. SAW RELIEF JOINTS AS THE ENGINEER DIRECTS OR APPROVES. REMOVE THE EXISTING JPC PAVEMENT TO THE LENGTH AND AT THE LOCATIONS NOTED ELSEWHERE IN THE CONTRACT. L=6 FEET MINIMUM AND LOCATIONS "J" SHALL NOT BE CLOSER THAN 6 FEET TO ANY TRANSVERSE JOINT BEYOND THE REPAIR.
2. INSTALL SMOOTH, LOAD TRANSFER DOWELS (EXCEPT USE TIE BARS FOR SECTION C), 18 INCHES LONG (SEE STANDARD DRAWING NO. RPS-020 FOR DOWEL SIZE) AT LOCATIONS "J". INSTALL DOWELS (OR TIE BARS FOR SECTION C) IN THE EXISTING CONCRETE USING EPOXY TYPE IV. INSTALL DOWELS (OR TIE BARS FOR SECTION C) ON 12 INCH CENTERS BEGINNING 12 INCHES FROM THE EDGE OF THE SLAB. IF L IS GREATER THAN 20 FEET, INSTALL NEW LOAD TRANSFER ASSEMBLY(S) AND CONSTRUCT CONTRACTION JOINTS SUCH THAT THE DISTANCE BETWEEN JOINTS IN THE REPLACED SECTION IS NO LESS THAN 10 FEET OR MORE THAN 20 FEET. TRANSVERSE JOINTS SHALL BE SPACED APPROXIMATELY 15' EQUIDISTANT, BUT NOT LESS THAN 10 FEET OR NO MORE THAN 20 FEET. ADJUST JOINTS TO PROVIDE THE MINIMUM NUMBER OF JOINTS WITHOUT EXCEEDING THE 10-20 FOOT RANGE. INSTALL NEW LOAD TRANSFER ASSEMBLY(S) AND ALIGN LOAD TRANSFER ASSEMBLY(S) WITH AN EXISTING JOINT OR CRACK IN THE ADJACENT SLAB IF ONLY ONE LANE IS BEING REPLACED.
3. IF ONLY ONE LANE IS REMOVED, AND L > 25', INSTALL NEW 1-INCH TIE BARS 18 INCHES LONG ON 30 INCH CENTERS IN THE LONGITUDINAL JOINT USING EPOXY TYPE IV. IF 2 OR MORE LANES ARE REMOVED, CONSTRUCT LONGITUDINAL JOINT(S) ACCORDING TO THE STANDARD DRAWING EXCEPT USE 1-INCH TIE BARS 18 INCHES LONG ON 30 INCH CENTERS. IF L < 25', DO NOT TIE THE LONGITUDINAL JOINT TO THE EXISTING LANE; USE A BOND BREAKER MATERIAL APPROVED BY THE ENGINEER THAT WILL ASSURE NO INTERACTION WITH THE ADJACENT LANE.
4. REPLACE WITH NON-REINFORCED JPC PAVEMENT AND INSTALL CONTRACTION JOINTS AT LOCATIONS "K" AND CONTRACTION JOINTS (OR A CONSTRUCTION JOINT FOR LOCATION C) AT LOCATIONS "J". SAW AND SEAL ALL JOINTS.
6. SEE "CROSS SECTION" FOR SECTION F.



PLAN VIEW

JPC PAVEMENT TO BE REMOVED



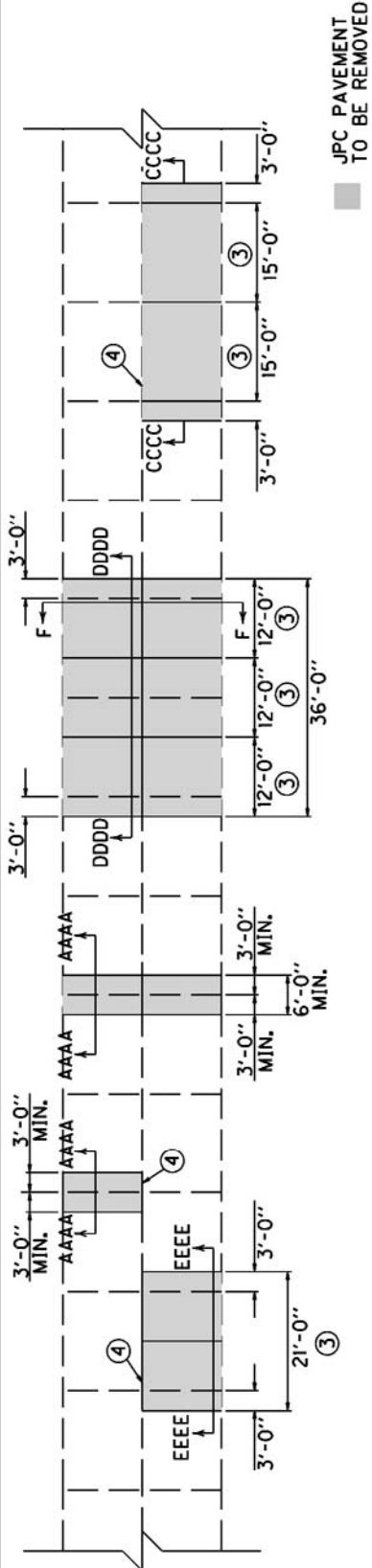
SECTION A
JOINT REPLACEMENT

SECTION C
LANE REPLACEMENT WHERE ADJACENT LANES OR JPC SHOULDERS WILL REMAIN

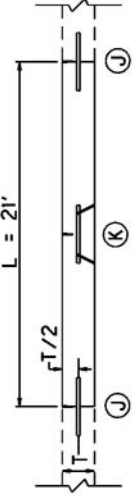
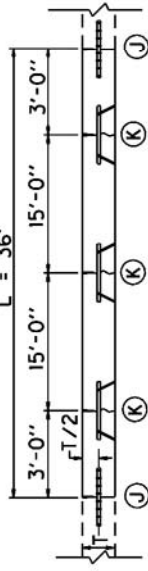
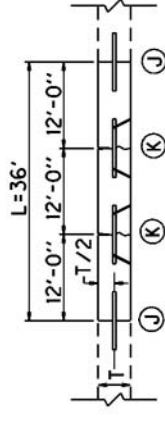


SECTION B
MID-SLAB REPLACEMENT

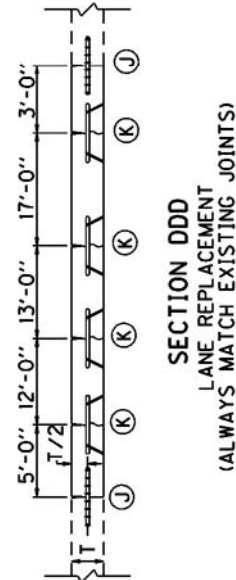
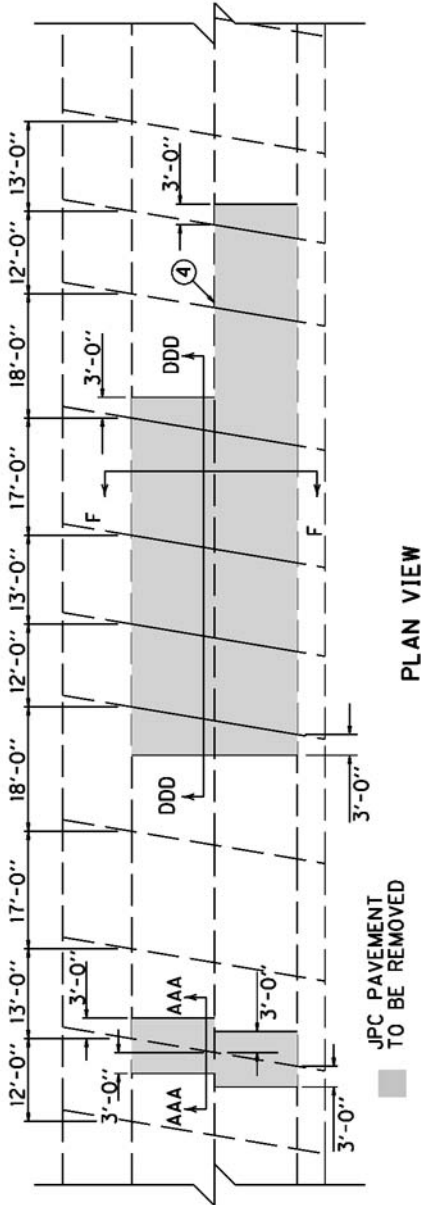
SECTION D
FULL WIDTH REPLACEMENT (INCLUDING JPC SHOULDERS)

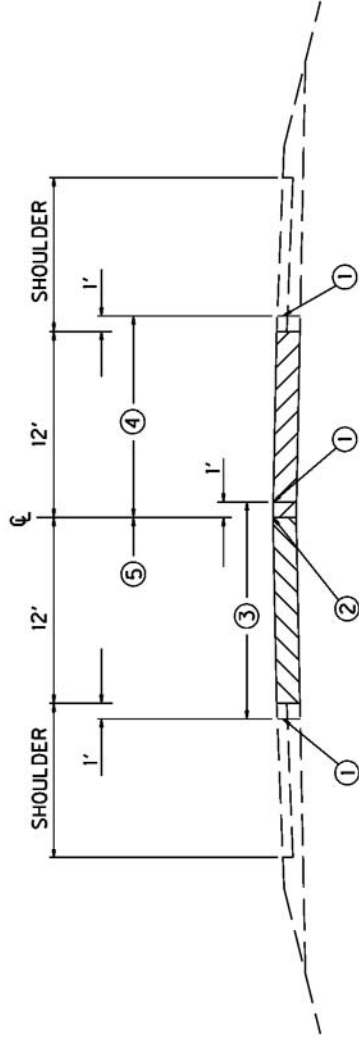


1. SAW AT LOCATIONS "J" AND ALONG LONGITUDINAL JOINT (IF ONLY ONE LANE IS REMOVED) FULL DEPTH WITHOUT DAMAGE TO EXISTING CONCRETE; SAW RELIEF JOINTS AS THE ENGINEER DIRECTS OR APPROVES. REMOVE THE EXISTING JPC PAVEMENT TO THE LENGTH AND AT THE LOCATIONS NOTED ELSEWHERE IN THE CONTRACT. L=6 FEET MINIMUM AND LOCATIONS "J" SHALL NOT BE CLOSER THAN 6 FEET TO ANY TRANSVERSE JOINT BEYOND THE REPAIR.
2. INSTALL SMOOTH, LOAD TRANSFER DOWELS (EXCEPT USE TIE BARS FOR SECTION CCCC); 18 INCHES LONG (SEE STANDARD DRAWING NO. RPS-020 FOR DOWEL SIZE) AT LOCATIONS "J". INSTALL DOWELS (OR TIE BARS FOR SECTION CCCC) IN THE EXISTING CONCRETE USING EPOXY TYPE IV. INSTALL DOWELS (OR TIE BARS FOR SECTION CCCC) ON 12 INCH CENTERS BEGINNING 12 INCHES FROM THE EDGE OF THE SLAB.
3. IF L IS GREATER THAN 20 FEET, INSTALL NEW LOAD TRANSFER ASSEMBLY(S) AND CONSTRUCT CONTRACTION JOINTS SUCH THAT THE DISTANCE BETWEEN JOINTS IN THE REPLACED SECTION IS NO LESS THAN 10 FEET OR MORE THAN 20 FEET. TRANSVERSE JOINTS SHALL BE SPACED APPROXIMATELY 15' EQUIDISTANT, BUT NOT LESS THAN 10 FEET OR NO MORE THAN 20 FEET. ADJUST JOINTS TO PROVIDE THE MINIMUM NUMBER OF JOINTS WITHOUT EXCEEDING THE 10-20 FOOT RANGE. INSTALL NEW LOAD TRANSFER ASSEMBLY(S) AND ALIGN LOAD TRANSFER ASSEMBLY(S) WITH AN EXISTING JOINT OR CRACK IN THE ADJACENT SLAB IF ONLY ONE LANE IS BEING REPLACED.
4. IF ONLY ONE LANE IS REMOVED, AND L > 25', INSTALL NEW 1-INCH TIE BARS 18 INCHES LONG ON 30 INCH CENTERS IN THE LONGITUDINAL JOINT USING EPOXY TYPE IV. IF 2 OR MORE LANES ARE REMOVED, CONSTRUCT LONGITUDINAL JOINT(S) ACCORDING TO THE STANDARD DRAWING EXCEPT USE 1-INCH TIE BARS 18 INCHES LONG ON 30 INCH CENTERS. IF L < 25', DO NOT TIE THE LONGITUDINAL JOINT TO THE EXISTING LANE; USE A BOND BREAKER MATERIAL APPROVED BY THE ENGINEER THAT WILL ASSURE NO INTERACTION WITH THE ADJACENT LANE.
5. REPLACE WITH NON-REINFORCED JPC PAVEMENT AND INSTALL CONTRACTION JOINTS AT LOCATIONS "K" AND CONTRACTION JOINTS (OR A CONTRACTION JOINT FOR LOCATION CCCC) AT LOCATIONS "J". SAW AND SEAL ALL JOINTS.
6. SEE "CROSS SECTION" FOR SECTION F.



1. SAW AT LOCATIONS "J" AND ALONG LONGITUDINAL JOINT (IF ONLY ONE LANE IS REMOVED) FULL DEPTH WITHOUT DAMAGE TO EXISTING CONCRETE. SAW RELIEF JOINTS AS THE ENGINEER DIRECTS OR APPROVES. REMOVE THE EXISTING JPC PAVEMENT TO THE LENGTH AND AT THE LOCATIONS NOTED ELSEWHERE IN THE CONTRACT. L=6 FEET MINIMUM AND LOCATIONS "J" SHALL NOT BE CLOSER THAN 6 FEET TO ANY TRANSVERSE JOINT BEYOND THE REPAIR.
2. INSTALL SMOOTH, LOAD TRANSFER DOWELS (EXCEPT USE TIE BARS FOR SECTION DDD), 18 INCHES LONG. (SEE STANDARD DRAWING NO. RPS-020 FOR DOWEL SIZE) AT LOCATIONS "J". INSTALL DOWELS (OR TIE BARS FOR SECTION DDD) IN THE EXISTING CONCRETE USING EPOXY TYPE IV. INSTALL DOWELS (OR TIE BARS FOR SECTION DDD) ON 12 INCH CENTERS BEGINNING 12 INCHES FROM THE EDGE OF THE SLAB. IF L IS GREATER THAN 20 FEET, INSTALL NEW LOAD TRANSFER ASSEMBLY(S) AND MATCH EXISTING JOINTS. INSTALL NEW LOAD TRANSFER ASSEMBLY(S) AND ALIGN LOAD TRANSFER ASSEMBLY(S) WITH EXISTING JOINTS IN ADJACENT SLABS.
- ④ IF ONLY ONE LANE IS REMOVED, AND L>25', INSTALL NEW 1-INCH TIE BARS 18 INCHES LONG ON 30 INCH CENTERS IN THE LONGITUDINAL JOINT USING EPOXY TYPE IV. IF 2 OR MORE LANES ARE REMOVED, CONSTRUCT LONGITUDINAL JOINT(S) ACCORDING TO THE STANDARD DRAWING EXCEPT USE 1-INCH TIE BARS 18 INCHES LONG ON 30 INCH CENTERS. IF L<25', DO NOT TIE THE LONGITUDINAL JOINT TO THE EXISTING LANE; USE A BOND BREAKER MATERIAL APPROVED BY THE ENGINEER THAT WILL ASSURE NO INTERACTION WITH THE ADJACENT LANE.
5. REPLACE WITH NON-REINFORCED JPC PAVEMENT AND INSTALL CONTRACTION JOINTS AT LOCATIONS "K". AND CONTRACTION JOINTS (OR A CONSTRUCTION JOINT FOR LOCATION DDD) AT LOCATIONS "J". SAW AND SEAL ALL JOINTS.
6. SEE "CROSS SECTION" FOR SECTION F.





SECTION F

- ① SAW-CUT LINE. THIS ONE FOOT IS TO ALLOW FOR A FORM AND THE REMOVAL AND REPLACEMENT SHALL BE INCIDENTAL TO THE WORK, EXCEPT NEW ASPHALT MIXTURE SHALL BE PAID DIRECT ON A TONNAGE BASIS, AND NEW JPC PAVEMENT WILL BE PAID BY THE SQUARE YARD. COMPACT THE DGA BASE BY MECHANICAL TAMPERS TO THE ENGINEER'S SATISFACTION.
- ② EXISTING LONGITUDINAL JOINT.
- ③ FIRST SLAB REMOVAL LIMITS AND REPLACE 12-FOOT LANE.
- ④ SECOND SLAB REMOVAL LIMITS AND REPLACE 12-FOOT LANE.
- ⑤ THIS ONE FOOT IS TO ALLOW FOR A FORM ON THE FIRST POUR, AND A TEMPORARY PAVEMENT IS REQUIRED. THE DEPARTMENT WILL NOT REQUIRE REMOVAL OF THIS ONE FOOT IF THE GRADE OF THE EXISTING PAVEMENT IS ADEQUATE TO ENSURE THE NEW CONCRETE CAN BE PLACED AND FINISHED TO THE SATISFACTION OF THE ENGINEER. ANY TEMPORARY PAVEMENT IS INCIDENTAL TO JPC PAVEMENT.
6. THE ABOVE DRAWING DEPICTS THE ORDER OF SLAB REMOVAL WHEN BOTH ARE TO BE REMOVED AT THE SAME LOCATION. WHEN ONLY ONE SLAB OR LANE IS TO BE REMOVED, REMOVE AND REPLACE ACCORDING TO SECTION C, CC, OR CCCC. TRAFFIC CONTROL WILL SPECIFY WHICH LANE TO REMOVE FIRST.

6-2021.00
Campbell County
I-471

Special Note for Cement Subgrade Stabilization (For Expedited Construction Locations)

DESCRIPTION

This note is to apply only to reconstruction of ramps that are to be constructed in either a 9-day or 16-day closure. This note will also apply to the expedited construction section of mainline I-471 between Sta. 282+96.77 and 297+00 (Ohio River Bridge). The existing subgrade is to be cement stabilized 12 inches deep on this project. In order to expedite construction, the curing time and testing of the stabilized subgrade has been approved to be subject to an experimental procedure. The subgrade is to be prepared and mixed per Section 208 of the Standard Specifications. Cement should be applied at a rate of 6% by weight at 108 lb/cu ft, or as directed by the Engineer based on field conditions. The curing time will be waived per the following items in this note.

- A. A 100 foot control strip will be completed at the beginning of stabilization of the subgrade. Density will be measured after each pass of the roller until no increase in density occurs. The target density will be established based on 95% of the maximum density achieved. Moisture content will be accepted based on visual observation.
- B. If the stabilized roadbed is not overlaid with aggregate base within 6 hours it is to be coated with an asphalt curing seal. This is to ensure that the proper moisture content is maintained during the shortened curing period.
- C. After properly mixing and compacting the stabilized base the contractor can begin constructing the aggregate base. Care should be taken by the Contractor to minimize the amount and weight of construction equipment operated on the stabilized base. If any disturbance or rutting in excess of ½ inch of the treated aggregate layer is detected, it must be repaired immediately. Repair of the stabilized aggregate is defined as re-rolling to refill any ruts or eliminate any bulges to ensure the depth of stabilized material is 12 inches and the stabilized layer is at proper grade. The asphalt curing seal must be reapplied over disturbed areas so that it completely covers the stabilized area.
- D. The Geotechnical Branch will coordinate with the district construction personnel to ensure that samples or in-situ test measurements of the stabilized base are taken. The rate of the sampling will be determined during stabilization to ensure enough are taken to be representative of the entire stabilized layer. These tests will be used for verification of the stabilized base strength. The Department may adjust cement or moisture content based on these strength measurements.

February 9, 2012

6-2021.00
Campbell County
I-471

PAYMENT

The Department will make payment for the completed and accepted quantities under the following:

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
0008	Cement Stabilized Roadbed	Square Yard
2542	Cement	Ton

CONTRACT ID: 121002
COUNTY: CAMPBELL
PROPOSAL: IM NH 4714(034)

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
SECTION 0001 PAVING NO ALTERNATE (REVISED: 2-13-12)						
0010	00001	DGA BASE	8,265.000	TON		
0020	00008	CEMENT STABILIZED ROADBED	30,990.000	SQYD		
0030	00018	DRAINAGE BLANKET-TYPE II-ASPH	4,890.000	TON		
0040	00022	JPC PAVEMENT DRAINAGE BLANKET	1,267.000	TON		
0050	00100	ASPHALT SEAL AGGREGATE	65.100	TON		
0060	00103	ASPHALT SEAL COAT	7.900	TON		
0070	00214	CL3 ASPH BASE 1.00D PG64-22	121.000	TON		
0080	00217	CL4 ASPH BASE 1.00D PG64-22	15,225.000	TON		
0090	00219	CL4 ASPH BASE 1.00D PG76-22	3,911.000	TON		
0100	00342	CL4 ASPH SURF 0.38A PG76-22	1,671.000	TON		
0110	00358	ASPHALT CURING SEAL	50.500	TON		
0120	02058	REMOVE PCC PAVEMENT	24,384.000	SQYD		
0130	02071	JPC PAVEMENT-11 IN	205.000	SQYD		
0140	02086	JPC PAVEMENT-13 IN	3,906.000	SQYD		
0150	02087	JPC PAVEMENT-13 IN SHLD	2,431.000	SQYD		
0160	02091	REMOVE PAVEMENT	2,065.000	SQYD		
0170	02115	SAW-CLEAN-RESEAL TVERSE JOINT	6,145.000	LF		
0180	02116	SAW-CLEAN-RESEAL LONGIT JOINT	5,924.000	LF		
0190	02200	ROADWAY EXCAVATION	60,453.000	CUYD		
0200	02235	BACKFILLING UNDERCUT	450.000	CUYD		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
0210	02542	CEMENT	724.000	TON		
0220	02598	FABRIC-GEOTEXTILE TYPE III	1,418.000	SQYD		
0230	02696	SHOULDER RUMBLE STRIPS-SAWED	4,328.000	LF		
0240	02702	SAND FOR BLOTTER	140.000	TON		
0250	03302	REPAIR CONCRETE CURB	93.000	LF		
0260	21173EC	SAW-CLEAN-RESEAL RANDOM CRACKS	133.000	LF		
ALT GROUPIAA10002		PAVING ASPHALT ALTERNATE				
0270	00001	DGA BASE	109,179.000	TON		
0280	00003	CRUSHED STONE BASE	999.000	TON		
0290	00008	CEMENT STABILIZED ROADBED	399,027.000	SQYD		
0300	00018	DRAINAGE BLANKET-TYPE II-ASPH	87,917.000	TON		
0310	00100	ASPHALT SEAL AGGREGATE	764.600	TON		
0320	00103	ASPHALT SEAL COAT	91.800	TON		
0330	00214	CL3 ASPH BASE 1.00D PG64-22	81,302.000	TON		
0340	00217	CL4 ASPH BASE 1.00D PG64-22	163,912.000	TON		
0350	00219	CL4 ASPH BASE 1.00D PG76-22	43,964.000	TON		
0360	00339	CL3 ASPH SURF 0.38D PG64-22	8,346.000	TON		
0370	00342	CL4 ASPH SURF 0.38A PG76-22	20,338.000	TON		
0380	00358	ASPHALT CURING SEAL	713.000	TON		
0390	01825	ISLAND CURB AND GUTTER	261.000	LF		
0400	01845	ISLAND INTEGRAL CURB	25.000	LF		
0410	01891	ISLAND HEADER CURB TYPE 2 (REVISED: 2-13-12)	9,031.000	LF		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
0420	02058	REMOVE PCC PAVEMENT	375,000.000	SQYD		
0430	02091	REMOVE PAVEMENT	5,244.000	SQYD		
0440	02200	ROADWAY EXCAVATION	96,143.000	CUYD		
0450	02235	BACKFILLING UNDERCUT	2,085.000	CUYD		
0460	02542	CEMENT	11,710.000	TON		
0470	02598	FABRIC-GEOTEXTILE TYPE III	6,568.000	SQYD		
0480	02599	FABRIC-GEOTEXTILE TYPE IV	4,349.000	SQYD		
0490	02696	SHOULDER RUMBLE STRIPS-SAWED	72,851.000	LF		
0500	02702	SAND FOR BLOTTER	1,980.000	TON		
0510	10203ND	PAVEMENT ADJUSTMENT ASPHALT	(1.00)	LS	2,084,493.00	2,084,493.00
0520	20997ED	REMOVE TRAFFIC ISLAND	65.000	SQYD		
0530	24459EC	CONCRETE MEDIAN BARRIER TYPE 9E2 (REVISED: 2-13-12)	1,493.000	LF		
ALT GROUPIAA20003		PAVING CONCRETE ALTERNATE				
0540	00001	DGA BASE	16,141.000	TON		
0550	00003	CRUSHED STONE BASE	71,158.000	TON		
0560	00008	CEMENT STABILIZED ROADBED	381,895.000	SQYD		
0570	00022	JPC PAVEMENT DRAINAGE BLANKET	16,481.000	TON		
0580	00100	ASPHALT SEAL AGGREGATE	764.600	TON		
0590	00103	ASPHALT SEAL COAT	91.800	TON		
0600	00214	CL3 ASPH BASE 1.00D PG64-22	2,225.000	TON		
0610	00339	CL3 ASPH SURF 0.38D PG64-22	325.000	TON		
0620	00358	ASPHALT CURING SEAL	381.900	TON		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
0630	01825	ISLAND CURB AND GUTTER	47.000	LF		
0640	01845	ISLAND INTEGRAL CURB (REVISED: 2-13-12)	9,021.000	LF		
0650	01891	ISLAND HEADER CURB TYPE 2	249.000	LF		
0660	02058	REMOVE PCC PAVEMENT	375,000.000	SQYD		
0670	02071	JPC PAVEMENT-11 IN	40,990.000	SQYD		
0680	02072	JPC PAVEMENT-11 IN SHLD	19,977.000	SQYD		
0690	02086	JPC PAVEMENT-13 IN	223,629.000	SQYD		
0700	02087	JPC PAVEMENT-13 IN SHLD	96,863.000	SQYD		
0710	02091	REMOVE PAVEMENT	5,244.000	SQYD		
0720	02200	ROADWAY EXCAVATION	23,306.000	CUYD		
0730	02235	BACKFILLING UNDERCUT	2,004.000	CUYD		
0740	02542	CEMENT	11,208.000	TON		
0750	02598	FABRIC-GEOTEXTILE TYPE III	6,312.000	SQYD		
0760	02599	FABRIC-GEOTEXTILE TYPE IV	3,778.000	SQYD		
0770	02695	RUMBLE STRIPS TYPE 3	8,710.000	LF		
0780	02702	SAND FOR BLOTTER	955.000	TON		
0790	10203ND	PAVEMENT ADJUSTMENT CONC	(1.00)	LS	1,437,067.00	1,437,067.00
0800	20997ED	REMOVE TRAFFIC ISLAND	65.000	SQYD		
0810	23335EC	CONCRETE MEDIAN BARRIER TY 9B2	1,493.000	LF		
SECTION 0004 ROADWAY						
0820	00071	CRUSHED AGGREGATE SIZE NO 57	1,541.000	TON		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
0830	00078	CRUSHED AGGREGATE SIZE NO 2	72.000	TON		
0840	00461	CULVERT PIPE-15 IN	129.000	LF		
0850	00462	CULVERT PIPE-18 IN	70.000	LF		
0860	00464	CULVERT PIPE-24 IN	30.000	LF		
0870	01001	PERFORATED PIPE-6 IN	95,384.000	LF		
0880	01002	PERFORATED PIPE-8 IN	7,594.000	LF		
0890	01011	NON-PERFORATED PIPE-6 IN	2,052.000	LF		
0900	01012	NON-PERFORATED PIPE-8 IN	44.000	LF		
0910	01015	INSPECT & CERTIFY EDGE DRAIN SYSTEM	(1.00)	LS		
0920	01021	PERF PIPE HEADWALL TY 1-6 IN	25.000	EACH		
0930	01022	PERF PIPE HEADWALL TY 1-8 IN	1.000	EACH		
0940	01025	PERF PIPE HEADWALL TY 2-6 IN	1.000	EACH		
0950	01029	PERF PIPE HEADWALL TY 3-6 IN	24.000	EACH		
0960	01033	PERF PIPE HEADWALL TY 4-6 IN	21.000	EACH		
0970	01456	CURB BOX INLET TYPE A	1.000	EACH		
0980	01490	DROP BOX INLET TYPE 1	1.000	EACH		
0990	01502	DROP BOX INLET TYPE 5A	1.000	EACH		
1000	01511	DROP BOX INLET TYPE 5D	1.000	EACH		
1010	01584	CAP DROP BOX INLET	2.000	EACH		
1020	01585	REMOVE DROP BOX INLET	1.000	EACH		
1030	01621	CONC MED BARR BOX INLET TY 9B1	2.000	EACH		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE UNIT QUANTITY	UNIT PRICE	AMOUNT
1040	01690	FLUME INLET TYPE 1	2.000 EACH		
1050	01691	FLUME INLET TYPE 2	12.000 EACH		
1060	01719	ADJUST INLET	1.000 EACH		
1070	01741	CORED HOLE DRAINAGE BOX CON-6 IN	93.000 EACH		
1080	01742	CORED HOLE DRAINAGE BOX CON-8 IN	4.000 EACH		
1090	01789	RECONSTRUCT MANHOLE	1.000 EACH		
1100	01982	DELINEATOR FOR GUARDRAIL-WHITE	632.000 EACH		
1110	01983	DELINEATOR FOR GUARDRAIL-YELLOW	41.000 EACH		
1120	01984	DELINEATOR FOR BARRIER - WHITE	3,448.000 EACH		
1130	01985	DELINEATOR FOR BARRIER - YELLOW	4,218.000 EACH		
1140	02003	RELOCATE TEMP CONC BARRIER	93,240.000 LF		
1150	02014	BARRICADE-TYPE III	25.000 EACH		
1160	02237	DITCHING	20,000.000 LF		
1170	02265	REMOVE FENCE	166.000 LF		
1180	02274	FENCE-6 FT CHAIN LINK	166.000 LF		
1190	02363	GUARDRAIL CONNECTOR TO BRIDGE END TY A	13.000 EACH		
1200	02369	GUARDRAIL END TREATMENT TYPE 2A	30.000 EACH		
1210	02373	GUARDRAIL END TREATMENT TYPE 3	7.000 EACH		
1220	02381	REMOVE GUARDRAIL	34,551.300 LF		
1230	02387	GUARDRAIL CONNECTOR TO BRIDGE END TY A-1	13.000 EACH		
1240	02391	GUARDRAIL END TREATMENT TYPE 4A	23.000 EACH		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
1250	02397	TEMP GUARDRAIL	550.000	LF		
1260	02483	CHANNEL LINING CLASS II	1,041.000	TON		
1270	02484	CHANNEL LINING CLASS III	120.000	TON		
1280	02555	CONCRETE-CLASS B	183.000	CUYD		
1290	02562	SIGNS	4,400.000	SQFT		
1300	02600	FABRIC GEOTEXTILE TY IV FOR PIPE	306.000	SQYD	2.00	612.00
1310	02650	MAINTAIN & CONTROL TRAFFIC	(1.00)	LS		
1320	02653	LANE CLOSURE	26.000	EACH		
1330	02671	PORTABLE CHANGEABLE MESSAGE SIGN	25.000	EACH		
1340	02690	SAFELOADING	4.000	CUYD		
1350	02720	SIDEWALK-4 IN CONCRETE	40.000	SQYD		
1360	02726	STAKING	(1.00)	LS		
1370	02775	ARROW PANEL	26.000	EACH		
1380	02894	CRASH CUSHION TYPE VI-T	29.000	EACH		
1390	02898	RELOCATE CRASH CUSHION	28.000	EACH		
1400	03148	CONC MEDIAN BARRIER TYPE 9E	910.000	LF		
1410	03171	CONCRETE BARRIER WALL TYPE 9T	64,100.000	LF		
1420	05950	EROSION CONTROL BLANKET	127,552.000	SQYD		
1430	05966	TOPDRESSING FERTILIZER	8.000	TON		
1440	05985	SEEDING AND PROTECTION (REVISED: 2-13-12)	11,600.000	SQYD		
1450	05989	SPECIAL SEEDING CROWN VETCH	14,340.000	SQYD		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
1460	06417	FLEXIBLE DELINEATOR POST-W	480.000	EACH		
1470	06418	FLEXIBLE DELINEATOR POST-Y	238.000	EACH		
1480	06511	PAVE STRIPING-TEMP PAINT-6 IN	380,000.000	LF		
1490	06513	PAVE STRIPING-TEMP PAINT-12 IN	16,400.000	LF		
1500	06549	PAVE STRIPING-TEMP REM TAPE-B	25,000.000	LF		
1510	06550	PAVE STRIPING-TEMP REM TAPE-W	25,000.000	LF		
1520	06551	PAVE STRIPING-TEMP REM TAPE-Y	25,000.000	LF		
1530	06568	PAVE MARKING-THERMO STOP BAR-24IN	191.000	LF		
1540	06573	PAVE MARKING-THERMO STR ARROW	2.000	EACH		
1550	06574	PAVE MARKING-THERMO CURV ARROW	20.000	EACH		
1560	06576	PAVE MARKING-THERMO ONLY	7.000	EACH		
1570	06585	PAVEMENT MARKER TY IVA-MW TEMP	4,352.000	EACH		
1580	06586	PAVEMENT MARKER TY IVA-MY TEMP	8,632.000	EACH		
1590	06592	PAVEMENT MARKER TYPE V-B W/R	1,879.000	EACH		
1600	06593	PAVEMENT MARKER TYPE V-B Y/R	651.000	EACH		
1610	08001	STRUCTURE EXCAVATION-COMMON	318.000	CUYD		
1620	08100	CONCRETE-CLASS A	4.660	CUYD		
1630	08150	STEEL REINFORCEMENT	178.000	LB		
1640	08904	CRASH CUSHION TY VI CLASS C TL3	1.000	EACH		
1650	10212ND	TIME COMPONENT NORTHBOUND; (REVISED: 2-13-12)	25,000.000	DOLL		
1660	10222ND	TIME COMPONENT SOUTHBOUND; (REVISED: 2-13-12)	50,000.000	DOLL		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
1670	20410ED	MAINTAIN LIGHTING	(1.00)	LS		
1680	21802EN	G/R STEEL W BEAM-S FACE (7 FT POST)	33,687.500	LF		
1690	21935EN	REMOVE CONC MEDIAN BARRIER	2,516.000	LF		
1700	23131ER701	PIPELINE VIDEO INSPECTION	65.000	LF		
1710	23143ED	KPDES PERMIT AND TEMP EROSION CONTROL	(1.00)	LS		
1720	23158ES505	DETECTABLE WARNINGS	115.000	SQFT		
1730	23237EN10W	WATERBLAST STRIPE REMOVAL	385,000.000	LF		
1740	23864EC	CHANNEL LINING CLASS III-MOD	365.000	TON		
1750	24189ER	DURABLE WATERBORNE MARKING-6 IN W	132,027.000	LF		
1760	24190ER	DURABLE WATERBORNE MARKING-6 IN Y	86,110.000	LF		
1770	24191ER	DURABLE WATERBORNE MARKING-12 IN W	23,795.000	LF		
1780	24458EC	UTILITY LINE HANGER FOR BRIDGES	2.000	EACH		
SECTION 0005 BRIDGE						
1790	02231	STRUCTURE GRANULAR BACKFILL	105.000	CUYD		
1800	02403	REMOVE CONCRETE MASONRY	40.800	CUYD		
1810	02555	CONCRETE-CLASS B	5.400	CUYD		
1820	02599	FABRIC-GEOTEXTILE TYPE IV	105.000	SQYD		
1830	02998	MASONRY COATING	500.000	SQYD		
1840	03294	EXPAN JOINT REPLACE 1 1/2 IN	432.000	LF		
1850	03295	EXPAN JOINT REPLACE 2 IN	124.000	LF		
1860	03298	EXPAN JOINT REPLACE 4 IN	475.000	LF		

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1870	03299	ARMORED EDGE FOR CONCRETE	37.000	LF		
1880	03300	ELIMINATE TRANSVERSE JOINT	415.000	LF		
1890	08001	STRUCTURE EXCAVATION-COMMON	230.000	CUYD		
1900	08016	REINF CONC SLOPE WALL-6 IN	283.000	SQYD		
1910	08020	CRUSHED AGGREGATE SLOPE PROT	135.000	TON		
1920	08033	TEST PILES	140.000	LF		
1930	08046	PILES-STEEL HP12X53	1,140.000	LF		
1940	08094	PILE POINTS-12 IN	44.000	EACH		
1950	08100	CONCRETE-CLASS A	144.200	CUYD		
1960	08104	CONCRETE-CLASS AA	125.800	CUYD		
1970	08150	STEEL REINFORCEMENT	18,074.000	LB		
1980	08151	STEEL REINFORCEMENT-EPOXY COATED	37,534.000	LB		
1990	08504	EPOXY SAND SLURRY	5,530.000	SQYD		
2000	08526	CONC CLASS M FULL DEPTH PATCH	156.000	CUYD		
2010	08534	CONCRETE OVERLAY-LATEX	1,695.000	CUYD		
2020	08549	BLAST CLEANING	37,482.000	SQYD		
2030	08550	HYDRODEMOLITION	32,515.000	SQYD		
2040	08551	MACHINE PREP OF SLAB	345.000	SQYD		
2050	08671	PRECAST PC BOX BEAM SB33	315.800	LF		
2060	21532ED	RAIL SYSTEM TYPE III	160.500	LF		
2070	23386EC	JOINT SEAL REPLACEMENT	1,031.000	LF		

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2080	23622EC	CLEAN DEBRIS FROM LOWER CHORD B00039L	(1.00)	LS		
2090	23622EC	CLEAN DEBRIS FROM LOWER CHORD B00039R	(1.00)	LS		
2100	24094EC	PARTIAL DEPTH PATCHING	336.200	CUYD		
2110	24424EC	REM AND REPLACE BRIDGE CHAIN LINK FENCE	604.000	LF		
2120	24438EC	SEAL CRACKS BRIDGE DECK B00056L	(1.00)	LS		
2130	24438EC	SEAL CRACKS BRIDGE DECK B00056R	(1.00)	LS		
2140	24439EC	SEAL BRIDGE DECK B00056L	(1.00)	LS		
2150	24439EC	SEAL BRIDGE DECK B00056R	(1.00)	LS		
2160	24456EC	EXPAN JOINT REPLACE 5 1/2 IN 5 1/2	102.000	LF		
SECTION 0006 SIGNING						
2170	04904	BARRIER MOUNTING BRACKET	2.000	EACH		
2180	06405	SBM ALUMINUM PANEL SIGNS	3,771.500	SQFT		
2190	06406	SBM ALUM SHEET SIGNS .080 IN	9.000	SQFT		
2200	06407	SBM ALUM SHEET SIGNS .125 IN	64.000	SQFT		
2210	06411	STEEL POST TYPE 2	90.000	LF		
2220	06441	GMSS GALV STEEL TYPE C	577.000	LB		
2230	06449	REM OVERHEAD SIGN SUPPORT STR	1.000	EACH		
2240	06450	REM OVERHEAD STRUC CONC BASE	1.000	EACH		
2250	06490	CLASS A CONCRETE FOR SIGNS	1.300	CUYD		
2260	06491	STEEL REINFORCEMENT FOR SIGNS	88.000	LB		
2270	20418ED	REMOVE & RELOCATE SIGNS	1.000	EACH		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
2280	20419ND	ROADWAY CROSS SECTION	1.000	EACH		
2290	20912ND	BARRIER WALL POST	2.000	EACH		
2300	21373ND	REMOVE SIGN PANEL	16.000	EACH		
SECTION 0007 SIGNALIZATION						
2310	04811	JUNCTION BOX TYPE B	7.000	EACH		
2320	04830	LOOP WIRE	3,575.000	LF		
2330	04850	CABLE-NO. 14/1 PAIR	2,100.000	LF		
2340	04895	LOOP SAW SLOT AND FILL	1,375.000	LF		
SECTION 0008 LIGHTING						
2350	04700	POLE 30 FT MTG HT	39.000	EACH		
2360	04701	POLE 40 FT MTG HT	98.000	EACH		
2370	04714	POLE 120 FT MTG HT HIGH MAST	18.000	EACH		
2380	04720	BRACKET 4 FT	18.000	EACH		
2390	04722	BRACKET 8 FT	18.000	EACH		
2400	04723	BRACKET 10 FT	30.000	EACH		
2410	04724	BRACKET 12 FT	47.000	EACH		
2420	04725	BRACKET 15 FT	14.000	EACH		
2430	04730	BRACKET C	12.000	EACH		
2440	04740	POLE BASE	98.000	EACH		
2450	04750	TRANSFORMER BASE	125.000	EACH		
2460	04760	POLE W/SECONDARY CONTROL EQUIP	1.000	EACH		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
2470	04761	LIGHTING CONTROL EQUIPMENT	4.000	EACH		
2480	04770	HPS LUMINAIRE	127.000	EACH		
2490	04771	HPS LUMINAIRE WALL PACK	3.000	EACH		
2500	04773	HPS LUMINAIRE HIGH MAST	107.000	EACH		
2510	04780	FUSED CONNECTOR KIT	284.000	EACH		
2520	04793	CONDUIT-1 1/4 IN	15,895.000	LF		
2530	04795	CONDUIT-2 IN	4,420.000	LF		
2540	04797	CONDUIT-3 IN	3,384.000	LF		
2550	04800	MARKER	58.000	EACH		
2560	04820	TRENCHING AND BACKFILLING	38,420.000	LF		
2570	04832	WIRE-NO. 12	21,399.000	LF		
2580	04833	WIRE-NO. 8	57,597.000	LF		
2590	04834	WIRE-NO. 6	20,910.000	LF		
2600	04835	WIRE-NO. 4	6,300.000	LF		
2610	04860	CABLE-NO. 8/3C DUCTED	2,120.000	LF		
2620	04861	CABLE-NO. 6/3C DUCTED	3,570.000	LF		
2630	04862	CABLE-NO. 4/3C DUCTED	17,466.000	LF		
2640	04863	CABLE-NO. 2/3C DUCTED	15,137.000	LF		
2650	04940	REMOVE LIGHTING	(1.00)	LS		
2660	20391NS835	JUNCTION BOX TYPE A	44.000	EACH		
2670	20392NS835	JUNCTION BOX TYPE C	9.000	EACH		

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2680	20993ND	HPS LUMINAIRE 400 WATT	12.000	EACH		
2690	21543EN	BORE AND JACK CONDUIT	4,104.000	LF		
2700	21563NN	SPLICE BOX 8 X 6 X 4 IN	10.000	EACH		
2710	22928EN	CABLE-NO. 1/3C DUCTED	3,612.000	LF		
2720	23161EN	POLE BASE-HIGH MAST	164.830	CUYD		
SECTION 0009 TRAFFIC LOOPS						
2730	04793	CONDUIT-1 1/4 IN	60.000	LF		
2740	04795	CONDUIT-2 IN	540.000	LF		
2750	04820	TRENCHING AND BACKFILLING	580.000	LF		
2760	04829	PIEZOELECTRIC SENSOR	50.000	EACH		
2770	04830	LOOP WIRE	25,310.000	LF		
2780	04895	LOOP SAW SLOT AND FILL	4,370.000	LF		
2790	20359NN	GALVANIZED STEEL CABINET	3.000	EACH		
2800	20360ES818	WOOD POST	6.000	EACH		
2810	20391NS835	JUNCTION BOX TYPE A	8.000	EACH		
2820	20392NS835	JUNCTION BOX TYPE C	2.000	EACH		
2830	21543EN	BORE AND JACK CONDUIT	120.000	LF		
SECTION 0010 DEMOB AND MOB						
2840	02568	MOBILIZATION (NO MORE THAN 5%)		LUMP		
2850	02569	DEMOBILIZATION (AT LEAST 1.5%)		LUMP		
TOTAL BID						